

NO REAL ESTATE
SALES TAX REQUIRED
COUNTY TREASURER

BY John Doe, Deputy

RECEIVED

SEP 18 1997

JEFFERSON COUNTY
PUBLIC WORKS DEPT.

AFTER RECORDING, RETURN TO:
Port Townsend City Clerk
City Hall
540 Water Street
Port Townsend WA 98368

Documents referred to herein: Auditor's File Number 403325

Abbreviated legal description: Ptn Tideland District 111 / Sec 10, T30N, R1W
Assessor's Tax Parcel No. 001104011

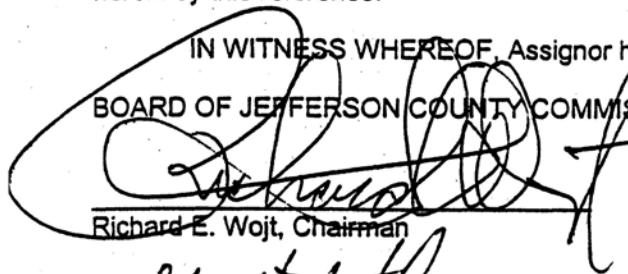
ASSIGNMENT OF EASEMENT

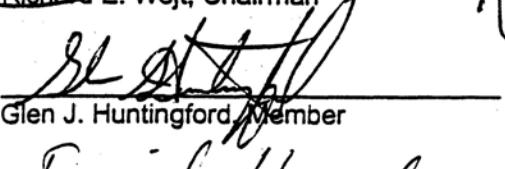
THIS ASSIGNMENT OF EASEMENT is made this 15th day of September, 1997, by JEFFERSON COUNTY, a Washington municipal corporation ("Assignor"), acting by and through its Board of County Commissioners, for the benefit of THE CITY OF PORT TOWNSEND, a Washington municipal corporation, and its assigns ("Assignee"), as follows:

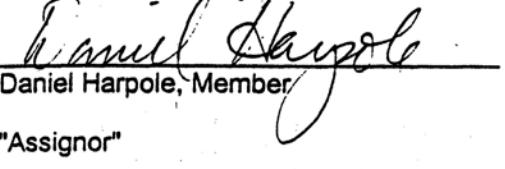
In consideration of and in accordance with the terms and conditions of an Interlocal Agreement recorded September 11, 1997, under Auditor's File Number 403325, in Volume 585, page 843, records of Jefferson County, Washington, which is incorporated herein by this reference, and for no monetary consideration, Assignor hereby assigns, sets over, and transfers to Assignee the right in the nature of an easement for public ingress, egress, and recreational and trail purposes granted and conveyed to Assignor by the Port of Port Townsend, a Washington municipal corporation, by instrument recorded August 29, 1997, under Auditor's File Number 403325, in Volume 585, page 843, records of Jefferson County, Washington, the terms and conditions of which are incorporated herein by this reference. The easement hereby assigned burdens certain real property owned by the Port of Port Townsend as described in Exhibit A attached hereto and incorporated herein by this reference.

IN WITNESS WHEREOF, Assignor has signed this instrument on the date first above written.

BOARD OF JEFFERSON COUNTY COMMISSIONERS

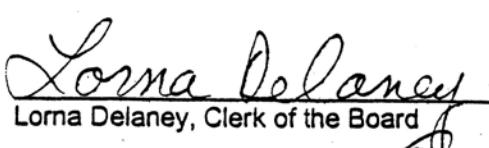

Richard E. Wojt, Chairman


Glen J. Huntingford, Member


Daniel Harpole, Member

"Assignor"

ATTEST:


Lorna Delaney, Clerk of the Board

APPROVED AS TO FORM:

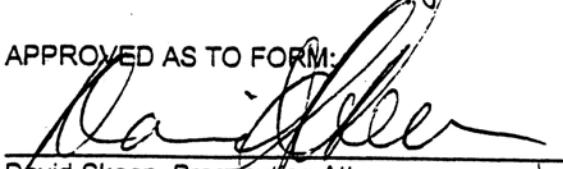

David Skeen, Prosecuting Attorney

EXHIBIT A

That portion of tideland District No. 111, first class tidelands of Port Townsend, Washington, lying within the following described parcel:

Commencing at the concrete monument marking the centerline intersection of Washington and Kearney Streets as said streets are shown on the Plat of L. B. Hastings 2nd Addition to Port Townsend recorded in Volume 1 of Plats, page 29, Records of Jefferson County, Washington;

Thence South 59°27'47" West, a distance of 1,172.00 feet to a concrete monument marking the centerline intersection of Washington and Benedict Streets in said Plat of L. B. Hastings 2nd Addition;

Thence South 62°26'43" West, a distance of 1,165.89 feet to the true point of beginning:

Thence North 27°53'40" West, a distance of 6.00 feet;

Thence South 62°06'20" West, a distance of 23.22 feet;

Thence South 41°54'11" West, a distance of 100.74 feet;

Thence South 17°15'51" West, a distance of 64.04 feet;

Thence South 28°15'38" East, a distance of 5.64 feet;

Thence South 61°44'22" West, a distance of 6.00 feet;

Thence South 28°15'38" East, a distance of 6.00 feet;

Thence South 61°44'22" West, a distance of 104.31 feet;

Thence South 76°19'48" West, a distance of 140.13 feet;

Thence South 87°27'58" West, a distance of 138.58 feet to the Northwestly boundary of that certain tract described in deed from the State of Washington to Washington Industrial Credit Corporation recorded May 19, 1928 in Volume 95 of Deeds, page 423, Records of Jefferson County, Washington.

Thence South 45°24'00" West along the Northwesterly boundary of said Washington Industrial Credit Corporation Tract, a distance of 17.91 feet;

Thence North 87°27'58" East, a distance of 153.05 feet;

Thence North 76°19'48" East, a distance of 154.74 feet;

Thence North 61°44'22" East, a distance of 69.32 feet;

Thence North 73°07'46" East, a distance of 34.78 feet;

Thence North 19°28'30" East, a distance of 8.24 feet;

Thence North 83°25'46" East, a distance of 46.72 feet;

Thence North 23°48'21" West, a distance of 36.21 feet;

Thence South 65°20'03" West, a distance of 44.83 feet;

Thence North 17°15'51" East, a distance of 58.23 feet;

Thence North 41°54'11" East, a distance of 95.98 feet;

Thence North 62°06'20" East, a distance of 21.08 feet;

Thence North 27°53'40" West, a distance of 6.00 feet to the true point of beginning.

RECORDED IN 802
VOL 586 PAGE 813
OF OFFICIAL RECORDS
REQUEST OF
J. County
87 SEP 11 PM 2:24
Public Works
DONNA H. ELDRIDGE
JEFFERSON COUNTY AUDITOR
BY Kariequin DEPUTY

403665

After Recording Return To:

Jefferson County Public Works
Post Office Box 2070
1322 Washington Street
Port Townsend, WA 98368

**Interlocal Agreement
Regarding
Public Trail Construction, Maintenance, and
Operation**

(Chapter 39.34 RCW)

GRANTORS:

City of Port Townsend
Jefferson County
Port of Port Townsend

GRANTEE:

City of Port Townsend
Jefferson County
Port of Port Townsend

SEP 11 1997

VOL 586 PAGE 802

**INTERLOCAL AGREEMENT REGARDING
PUBLIC TRAIL CONSTRUCTION, MAINTENANCE
AND OPERATION
(Chapter 39.34 RCW)**

THIS INTERLOCAL AGREEMENT is entered into this 18 day of August, 1997 by and between the Port of Port Townsend, a Washington municipal corporation (hereinafter "the Port"), Jefferson County, a Washington municipal corporation (hereinafter "the County"), and the City of Port Townsend, a Washington municipal corporation (hereinafter "the City"). The Port, the County and the City are collectively referred to herein as "the Parties."

A. RECITALS.

1. The Port has granted to the County an easement for the purposes of constructing and maintaining a portion of a pedestrian and bicycle trail, hereinafter known as "The Larry Scott Memorial Trail" or "Trail." The City has agreed to accept, and will accept, assignment of the easement to the City, on condition that the County fund construction of the Trail. A copy of the Easement, including legal descriptions of the easement area, is attached hereto as **ATTACHMENT A**. Additionally, the Port Townsend Paper Company has dedicated and conveyed the abandoned/unused railroad right of way running adjacent to Port Townsend Bay, situated within the City's municipal boundaries, to the City for Trail construction. It is the intent of this Interlocal Agreement to document the agreements between the Parties regarding the design, construction, development and maintenance of the Trail; and

2. The Port, the County and the City are tax-supported public entities, acting with a common purpose of best serving the interests and needs of the citizens, residents and taxpayers of the Jefferson County. In best serving the interests of the public, and in accordance with Chapter 39.34 RCW, the Parties desire to make the most efficient use of their municipal powers by cooperating with each other on a basis of mutual advantage to accord best with the needs and economic development of the Jefferson County community; and

3. The Parties mutually agree that a pedestrian trail is an important public amenity which will enhance access to the Port Townsend and Jefferson County shoreline and provide an essential linkage between the Jefferson County and Port Townsend communities, and will enhance recreational use and enjoyment of shoreline areas for the community. A Trail is an essential element in successfully implementing and coordinating the Port Townsend Urban Waterfront Plan, the Jefferson County and Port Townsend Shoreline Master Programs, and the Jefferson County and Port Townsend GMA Comprehensive Plans; and

4. The Parties share a common interest in establishing a multi-use pedestrian Trail within the agreed easement area (**ATTACHMENT A**) situated upon the Port's Boat Haven property. The Parties intend to construct the Trail to link areas to the north and south of the Boat Haven through development of a continuous waterfront pedestrian "waterwalk" trail system, connected to the Olympic Discovery Trail, situated in unincorporated Jefferson County. The Parties further agree that it is essential to take all reasonable measures to ensure that the Trail does not interfere with Port operations, particularly as uses of the Boat Haven property evolve with construction of Enhanced Haulout and other improvements to the Port's Boat Haven property; and

5. This Agreement addresses issues related to the development, construction, use and maintenance of portions of the Trail situated within the municipal boundaries of the City of Port Townsend and shall not control or pertain to the development, construction, use and maintenance of portions of the Trail situated outside the City's municipal boundaries. The intent of this Interlocal Agreement is to provide for the public recreational use of the Trail and to provide for reasonable, shared responsibility for development, operation and maintenance of the Trail,

NOW THEREFORE, for and in consideration of the above Recitals and for other good and valuable consideration, for the public purpose of constructing and operating a public waterfront pedestrian access trail, and for the purposes of maintaining the Larry Scott Memorial Trail through real property owned by the Port of Port Townsend, Jefferson County and the City of Port Townsend, the parties agree as follows:

B. AGREEMENT.

1. Trail Design. The Parties have worked together with the assistance of design professionals to craft a Trail design which they believe best serves the purposes stated in the above Recitals. Attached hereto as **ATTACHMENT B** is the design of relevant sections of the Trail, including the Washington and Boat Street intersection. The Parties agree to this design and direct staff to resolve remaining design concerns. The County will install signs in the locations, and with the initial content and design as depicted in **ATTACHMENT B**. The signs shall clearly advise Trail users when they are leaving the Trail area and entering the Port's industrial area. The Parties shall continue to resolve ongoing signage issues as the need for signs becomes better known to the Parties. In the event any Party believes that additional signage is necessary at any time in the future, the Parties agree to promptly discuss such need, and the County will prepare and install mutually agreed signs. The Parties shall mutually agree to the design, content and location of all signs, including kiosk(s).

2. Conveyance of Properties. The Port has conveyed an easement to the County in the form of **ATTACHMENT A** hereto, which easement is incorporated herein by this reference. The County has assigned, or will assign, the easement to the City (by donation), conveying to the City all real property interests of the County in the easement dedication area. The City has accepted conveyances from the Port Townsend Paper Company of sections of the Trail which are abandoned or unused railroad rights of way, including the railroad transfer span property which is subject to a Department of Natural Resources lease. Legal descriptions of the Port Townsend Paper Company properties are attached hereto as **ATTACHMENT C**. Immediately upon execution of this Agreement, the Port agrees to convey, by quit claim deed, any interest it may have in properties conveyed by the Port Townsend Paper Company to the City which will be used exclusively for the Trail, including the full width of the properties upon which the Trail is situated.

3. Purpose of Easement; Use of Trail. The purpose of the Trail, including but not limited to the easement over Port property (**ATTACHMENT A**) is to allow pedestrian and bicycle access (and such other users as may be agreed between the Parties) and entry onto the dedicated area by the general public and all members thereof for their peaceful enjoyment of the dedicated area and the waters of Port Townsend Bay adjoining the dedicated pedestrian and bicycle trail area. Equestrian use of the Trail is permitted through the sections of the Trail described in **ATTACHMENT C** (Port Townsend Paper Company conveyance properties) but shall be prohibited in all sections of Trail situated in the Port's Boat Haven property, described in **ATTACHMENT A**. The County shall make reasonable provisions and provide appropriate signs, approved by the Parties, to require and allow equestrian users to turn around prior to entering the Boat Haven area, as depicted on **ATTACHMENT B**.

3.1 The term "pedestrian" shall have its common meaning, and shall include, without limitation, motorized and non-motorized wheel chairs used by physically disabled persons, bicycles, and infant strollers, and such other users as the Parties may agree. With the exception of vehicle access for trail maintenance, trail access and use by motor vehicles, as defined in RCW 46.04.320, .330. and .332, is prohibited.

3.2 The public access dedication provides an essential component of the Larry Scott Memorial Trail, and will allow connection with additional public access trails, walkways and points on adjoining properties which may exist now or in the future.

4. Public Use of Trail -- Limitations.

4.1 The Trail is for the public recreational and pedestrian circulation uses and needs stated above. With the exception of maintenance and construction vehicles and motorized wheel chairs, access to the Trail by motorized vehicles will be specifically prohibited.

4.2 The Trail may be closed by the City or the County (including authorized employees) for reasonable periods from time to time on a temporary, short-term basis for maintenance or repairs to the Trail area.

4.3 The Port (including authorized employees) may close the Trail for limited periods to enable the enhanced haulout lift and other vehicles to cross the Trail area at the eastern edge of the Park, situated near the terminus of the Trail, to be formally named at a later date (by mutual agreement), but referred to herein as "Trail's End Park," as depicted on **ATTACHMENT B**. Such Trail crossings will be necessary and may be frequent for truck and crane access to the pier and for infrequent (but of potentially long duration) enhanced haulout lift and other vehicle crossings of the Trail area to the west of the Trail's End Park. The Port may construct, at its exclusive cost and expense, a ramp upon one section of the Trail to enable heavy vehicle crossings for boat launch purposes, such ramp not to exceed sixty (60) feet in width. The actual location of enhanced haulout lift and other vehicle crossings, including the heavy vehicle crossing ramp, will be determined in consultation with the Washington State Department of Ecology and other agencies with jurisdiction.

5. Construction Costs and Responsibilities. The County has secured, and believes it will continue to secure, State of Washington grant funding for the construction of the Trail. The City is participating with the County in applying for grant funding for construction of those sections of the Trail situated within the City's municipal boundaries.

5.1 The County shall be solely responsible for construction costs of all portions of the Larry Scott Memorial Trail situated within the City's municipal boundaries from available grant funds. Where the City has procured grant funding for Trail construction, the County shall act as the City's general contractor for the construction of the Trail, funded by grant funds received by the City. With the exception of restroom construction, if the grant funding for Trail construction is lost after execution of this Agreement, continued funding of the project is at the discretion of the Board of County Commissioners.

5.2 The County's construction costs shall include but not be limited to all engineering, permitting, design and construction costs for the Trail and shall also include all costs for the repair of any rip rap adjacent to the Trail area (as needed), and construction of a fence along the upland side of the Trail to separate the pedestrian walkway from the Port of Port Townsend Boat Haven industrial area. The County shall be solely and separately responsible for complying with any requirements for environmental mitigation measures imposed by the City, the State, or other regulatory body, to mitigate environmental impacts which may be associated with Trail construction. The County will be responsible for providing any and all measures which may be required to mitigate any adverse effects on the wetlands adjacent to the Trail, associated with Trail development, as identified at the date of

the execution of this Agreement; *provided, however* this Agreement shall not be construed to limit the City's responsibility and authority under any environmental laws.

5.3 The County shall be solely responsible for management, control and payment of its agents, contractors and employees engaging in any work of any nature in the engineering, permitting, design and construction of the Trail. The County agrees to defend, indemnify and hold the City and the Port harmless from any claims or causes of action associated with or arising during the construction phase from the engineering, permitting, design and construction of the Trail.

6. Operation and Maintenance Obligations and Costs. The parties agree to share responsibilities for the costs associated with the ongoing use, operation and maintenance of the Larry Scott Memorial Trail through the easement dedication area and abandoned railroad bed area (**ATTACHMENTS A & C**). Such operation and maintenance costs shall include, but not be limited to, the following:

6.1 Jefferson County Responsibilities. The County will be solely responsible for maintaining the Trail surface and pavement and all other improvements not specifically provided for City or Port maintenance, including but not limited to all surface repair and cleaning. Specifically, the County shall be responsible for all portions of the Trail to the west of the Trail's End Park, as depicted on **ATTACHMENT B**. In accordance with RCW 35.77.020, the City grants the County permission to access and maintain all portions of the Trail situated within the City's municipal boundaries.

6.2 City of Port Townsend Responsibilities. The City will be solely responsible for maintenance of landscaping, including weed control and common grass control; litter control and garbage removal; fence repairs; pet waste removal; and maintenance of the benches and other furnishings, signs (including kiosk signs), and bike racks in the Trail's End Park area, and the section of the Trail to the east of the Trail's End Park, as depicted on **ATTACHMENT B**.

6.3 Port of Port Townsend Responsibilities. The Port shall be solely responsible for maintenance and upkeep of the restrooms along the Trail area located within the Boat Haven. The Port agrees to address any impacts upon the Trail and Trail users associated with the wash down spray facilities, as needed.

7. Restroom Facilities. The parties agree that the construction and maintenance of restroom facilities as depicted on **ATTACHMENTS B and D** is an important aspect of the successful operation of the Trail. The restroom shall be designed generally as depicted in **ATTACHMENT D**, with all fixtures as shown, with the exception that the shower facilities are at the option of the Port, as provided herein. Prior to commencement of construction, the

parties shall mutually agree to the final design of the restroom. The parties agree to cooperate and jointly participate in application for all available grants to fund construction expenses. To the extent grant funds are not available, the City and the County shall jointly share the non-grant-funded portion of the construction expenses for the restroom, as provided in Paragraph 7.1 herein. In the event grant funds are not available for the entire restroom construction expenses in 1997, the County and the City will allocate funds to pay for restroom construction, said construction to be completed not later than during the 1999 summer months.

7.1 Responsibility for Construction Expense. The Port shall be responsible for construction of the restroom, from funds provided by the County and the City as stated herein. The Parties stipulate and agree that the cost to the City and the County for restroom facilities (not including optional shower facilities and associated expenses), designed in general conformance with **ATTACHMENT D**, shall not exceed a total of \$85,000, adjusted from the date of this Agreement by three percent (3%) pre annum to reflect the anticipated cost of inflation. Any cost for construction in excess of \$85,000 shall be the obligation of the Port. To the extent grant funds are not available for the restroom facilities, the City and the County shall share equally the non-grant-funded portion of restroom construction expenses; *provided, however,* the City's share of such expense shall not exceed \$20,000. The County shall be obligated to fund the balance of construction costs, not to exceed \$65,000. The restroom shall include separate toilet facilities for males and females and shall comply with Uniform Building Code (UBC) standards. In the event the Port desires construction of shower facilities for the use by Port patrons, and if grant funds are not available for the shower facilities, the Port shall be solely responsible for all costs associated with the shower facilities, including the proportionate share of costs of restroom construction attributable to the shower facilities, including but not limited to all plumbing, fixtures and floor area square footage.

7.2 Ownership of Restroom. The restroom will be the property of the Port of Port Townsend.

7.3 Public Access Schedule. The restroom shall be open to the public during the same hours as other restrooms maintained by the Port, situated on Port properties.

8. Recreational Use of Property. The parties intend through this Agreement to make publicly-owned property available for public recreational use pursuant to RCW 4.24.200 and RCW 4.24.210. It is understood and agreed that in making this public property available for recreational use, the Parties make no representations of any kind to any person, persons or entities regarding the safety of the Trail, and that this Agreement shall not be interpreted as establishing any benefit or liability for or in favor of any third party, against the Parties to this Agreement.

9. Duration of Agreement. The parties intend that this Agreement shall continue in force and effect so long as the Parties continue to develop, operate and maintain the Trail for public use. As such, this Agreement shall have an indefinite duration and shall expire only upon the mutual concurrence between the Parties that a substantial change of circumstances has occurred which mandates abandonment of this Agreement.

10. Entire Agreement; Amendments. This Agreement represents the entire and integrated agreement between the Parties. This Agreement may be amended only by a written instrument approved by all Parties pursuant to regularly-adopted resolutions or other appropriate instruments at open, public legislative meetings, and signed by authorized representatives of all Parties.

11. Entire Understanding; Severability and Waiver. This Agreement is the entire understanding between the parties and no changes, amendments, or alterations shall be effective unless agreed to in writing by the Parties. In the event any provision of this Agreement is rendered invalid or unenforceable, the remainder of this Agreement shall remain in full force and effect. In the event that any Party waives any provision of this Agreement, it shall not be deemed to have waived that provision at any other time or to have waived any other provision.

12. Assignability. No assignment by any Party of this Agreement, or any portion thereof, shall be permitted unless the assignment is authorized by a written instrument approved by all Parties pursuant to regularly-adopted resolutions or other appropriate instruments at open, public legislative meetings, and signed by authorized representatives of all Parties.

13. Indemnification. The parties expressly agree that each party shall defend, indemnify, and hold harmless the other parties, and their officers, officials, agents, employees, and volunteers from any and all claims, injuries, actions, damages, losses or suits including reasonable attorney's fees, which arise out of, or are connected with, or due to any errors, omissions, or negligent acts in performance of this Agreement, except for each party's own comparative negligence.

14. Representatives; Notices. For purposes of administration of this Agreement, the representatives of the Port, the City and the County are set forth in this section below. Any notice shall be delivered personally or may be mailed by certified mail, return receipt requested, to the other parties. In the case of notice by mail, notice shall be deemed given on the date of postmark of the return receipt.

PORt OF PORT TOWNSEND:

Assistant Port Manager
PO Box 1180
333 Benedict Street
Port Townsend, WA 98368
Telephone: (360) 385-0656 Fax: (360) 385-3988

CITY: City Administrator
City Hall
540 Water Street
Port Townsend, WA 98368
Telephone: (360) 379-1586 Fax: (360) 385-5248

JEFFERSON COUNTY:

Public Works Director
PO Box 2070
1322 Washington Street
Port Townsend, WA 98368
Telephone: (360) 385-9160 Fax: (360) 385-9172

15. Nondiscrimination. During the performance of this Agreement, none of the Parties nor their agents, subcontractors or employees, shall discriminate on the basis of race, color, sex, religion, national origin, creed, marital status, age, or the presence of any sensory, mental, or physical handicap in employment or application for employment or in the administration or delivery of services or any other benefits under the Agreement. The Parties shall comply with all applicable federal, state, or local laws, executive orders, and regulations.

16. Applicable Law; Venue. This Agreement shall be governed by the laws of Washington State. Venue for any action under this Agreement shall be in Jefferson County, Washington.

17. Binding Arbitration. The parties agree that any dispute arising out of the conduct of this Agreement shall be submitted to binding arbitration. Arbitration shall be conducted pursuant to the arbitration rules of the American Arbitration Association (AAA) and any judgment upon the award rendered by the arbitrator or arbitrators may be entered in the Jefferson County Superior Court. Arbitration shall be commenced when 45 days have passed following a written notice of dispute delivered by any Party to this Agreement.

18. Successors and Assigns. This Agreement shall be binding upon the parties, their heirs, personal representatives, successors and assigns.

19. Corporate Authority. Each individual executing this Agreement on behalf of the municipal corporation represents and warrants he/she is duly authorized to execute and deliver this Agreement on behalf of the municipal corporation in accordance with a duly and regularly-adopted resolution or other appropriate instrument adopted at an open, public legislative meeting, and that this Agreement is binding upon the municipal corporation in accordance with its terms.

IN WITNESS WHEREOF the Port of Port Townsend and Jefferson County have executed this *Interlocal Agreement Regarding Public Trail Construction, Maintenance and Operation* as of the date and year last shown below.

POR T OF PORT TOWNSEND:

POR T OF PORT TOWNSEND
BOARD OF COMMISSIONERS

By: 
Herbert F. Beck, President

Date: 6/25, 1997

By: 
Conrad W. Pirner, Vice President

Date: June 25, 1997

By: 
Robert H. Sokol, Secretary

Date: 6/25, 1997

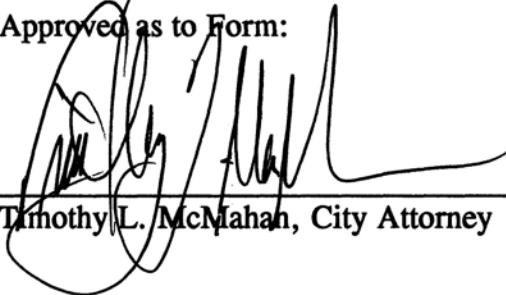
INTERLOCAL AGREEMENT

THE CITY OF PORT TOWNSEND:

By: 
Julie McCulloch, Mayor

Date: Aug 22, 1997

Approved as to Form:


Timothy L. McMahan, City Attorney

JEFFERSON COUNTY:

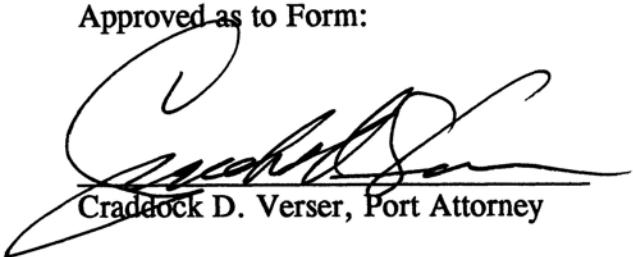
JEFFERSON COUNTY BOARD OF
COMMISSIONERS

By: 
Richard Wojt, Chair

PAGE 9

Date: Aug 15, 1997

Approved as to Form:



Craddock D. Verser, Port Attorney

By: Dan Harpole
Dan Harpole, Member

Date: 8/18/97, 1997

By: Glen Huntingford
Glen Huntingford, Member

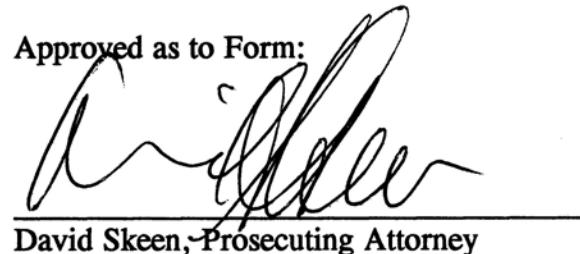
Date: _____, 1997

Attest:



Lorna Delaney
Lorna Delaney, County Clerk of the Board

Approved as to Form:



David Skeen, Prosecuting Attorney

ATTACHMENTS SHOWN BELOW ON FILE AT JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS

- A EASEMENT FROM PORT TO THE COUNTY/CITY,
INCLUDING LEGAL DESCRIPTION OF TRAIL AREA**
- B PLAN/MAP OF TRAIL DESIGN, INCLUDING SIGN
CONTENT**
- C PORT TOWNSEND PAPER COMPANY CONVEYANCE
TO THE CITY — LEGAL DESCRIPTION**
- D RESTROOM DESIGN — GENERAL**
- E REQUIRED CONDITIONS OF MITIGATION**
- F HYDRAULIC PROJECT APPROVAL**
- G MEMORANDUM OF UNDERSTANDING REGARDING
PURPOSE AND ARRANGEMENT FOR THE TRANSFER
OF REAL PROPERTY**

06/20/97Agree{LSMTMEM.UND}

NO REAL ESTATE
SALES TAX REQUIRED
COUNTY TREASURER

BY *J. Leachford / Deputy*

403325

585 843 - 845
OF CASH RECORDS
RECEIVED

97 AUG 29 PM 4:49

DONNA H. ELDRIDGE
JEFFERSON COUNTY AUDITOR

BY _____ DEPUTY

EASEMENT

IN THE MATTER OF JEFFERSON COUNTY ROAD PROJECT CR 1069-THE LARRY SCOTT MEMORIAL TRAIL:

THIS EASEMENT DOCUMENT SUPERSEDES THE EASEMENT DOCUMENT FILED UNDER AUDITOR FILE NUMBER 394986, VOLUME 561, PAGE 172-173. DATED SEPTEMBER 26, 1996.

KNOW ALL THESE MEN BY THESE PRESENTS, that the GRANTOR, the PORT OF PORT TOWNSEND, for \$32,000, in hand paid, and other good and valuable consideration, grants and conveys unto Jefferson County, or its assigns, a perpetual easement for the following purposes:

public ingress, egress, and recreational and trail purposes

over, under, across, and upon real property, situated in Jefferson County, State of Washington, described as follows:

That portion of tideland District No. 111, first class tidelands of Port Townsend, Washington, lying within the following described parcel:

Commencing at the concrete monument marking the centerline intersection of Washington and Kearney Streets as said streets are shown on the Plat of L. B. Hastings 2nd Addition to Port Townsend recorded in Volume 1 of Plats, page 29, Records of Jefferson County, Washington;

Thence South 59°27'47" West, a distance of 1,172.00 feet to a concrete monument marking the centerline intersection of Washington and Benedict Streets in said Plat of L. B. Hastings 2nd Addition;

Thence South 62°26'43" West, a distance of 1,165.89 feet to the true point of beginning;

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Thence South 61°44'22" West, a distance of 104.31 feet; 001104013, 001104016

Thence South 76°19'48" West, a distance of 140.13 feet;

Thence South 87°27'58" West, a distance of 138.58 feet to the Northwestly boundary of that certain tract described in deed from the State of Washington to Washington Industrial Credit Corporation recorded May 19, 1928 in Volume 95 of Deeds, page 423, Records of Jefferson County, Washington.

Thence South 45°24'00" West along the Northwesterly boundary of said Washington Industrial Credit Corporation Tract, a distance of 17.91 feet;

Thence North 87°27'58" East, a distance of 153.05 feet;

Thence North 76°19'48" East, a distance of 154.74 feet;

Thence North 61°44'22" East, a distance of 69.32 feet;

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Thence South 65°20'03" West, a distance of 44.83 feet;

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Thence North 62°06'20" East, a distance of 21.08 feet;

Thence North 27°53'40" West, a distance of 6.00 feet to the true point of beginning.

It is understood and agreed that the delivery of this Easement is hereby tendered and that the terms and obligations hereof shall not become binding upon Jefferson County unless and until accepted and approved hereon in writing for Jefferson County Board of County Commissioners. This

Easement is granted in perpetuity. Until execution of the Interlocal agreement, the Port shall have the right to cross the easement area for access purposes. However, construction or use of the Larry Scott Memorial Trail will not commence until an Interlocal Agreement is signed by all parties.

SUBJECT TO THE FOLLOWING:

1. It is understood and agreed by all parties that a portion of said easement is encumbered by the corner of a future Port of Port Townsend building, as defined in Exhibit A attached hereto.
2. It is further understood and agreed that a portion of said easement may be utilized by the Port of Port Townsend to construct a boat ramp, approximately 60 feet wide.

STATE OF WASHINGTON)
) ss
COUNTY OF JEFFERSON)

On this day personally appeared before me, Lloyd W. Cahoon, to me known to be the individual described in and who executed the within and foregoing instrument, and acknowledged that she signed the same as her free and voluntary act and deed, for the uses and purposes therein mentioned.
hus

GIVEN under my hand an official seal this 15th day of August, 1997.

Naomi M. Nobles
Notary Public in and for the State of Washington,
residing at Chimacum, WA

Dated this 15th day of August, 1997

By: [Signature]
Grantor

BOARD OF JEFFERSON COUNTY COMMISSIONERS' SEAL:

Richard E. Wojt
Richard E. Wojt, Chairman

Aug 18/97
Date

Glen J. Huntingford
Glen J. Huntingford, Member

Date

Daniel Harpole
Daniel Harpole, Member

Date

ATTEST:

Lorna Delaney
Lorna Delaney, Clerk of the Board

8/18/97
Date

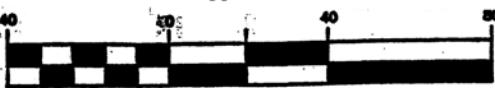
Klara A. Fabry
Klara A. Fabry, Public Works Director/County Engineer

8-18-97
Date

APPROVED AS TO FORM:

David Skeen
David Skeen, Prosecuting Attorney

8/15/97
Date



1 inch = 40 ft.

DATE OF FIELD SURVEY: 5/22 - 5/27/97.

LOCATION OF EASEMENT AS SHOWN IS APPROXIMATE; BASED
ON AN AUTOCAD DRAWING PROVIDED BY CLARK LAND OFFICE.

LOCATION OF ALL UNDER CONSTRUCTION CONCRETE SLABS, UTILITIES,
ETC. ARE APPROXIMATE; FIELD DATA WAS COLLECTED BY JEFFERSON
COUNTY ENGINEERING STAFF. HORIZONTAL DATUM IS BASED ON NAD 83/
1986 ADJUSTMENT AND FROM JEFFERSON COUNTY 1991 GPS PHOTO
CONTROL SURVEY, PER CONSTRUCTION PLANS FOR THE PORT
OF PORT TOWNSEND ENHANCED HAULOUT/STORMWATER FACILITIES
PROJECT PREPARED BY REID MIDDLETON.

▲ DENOTES CONTROL POINT

FIRE HYDRANT
TOP NUT=16.48

EXISTING
EASEMENT

CONCRETE SLAB

VULT

Z

WATER
VALVES

WATER
VALVES

Z

EXISTING
EASEMENT

CONC.
SLAB

DRAIN VULT
7.0' DIAM.

ELECTRICAL PANEL
TRANSFORMER

PAD

CATCH BASIN

RIP RAP

RIP RAP

EXHIBIT A



*Jefferson County
Department of Public Works*

**1322 Washington Street
Port Townsend • Washington • 98368**



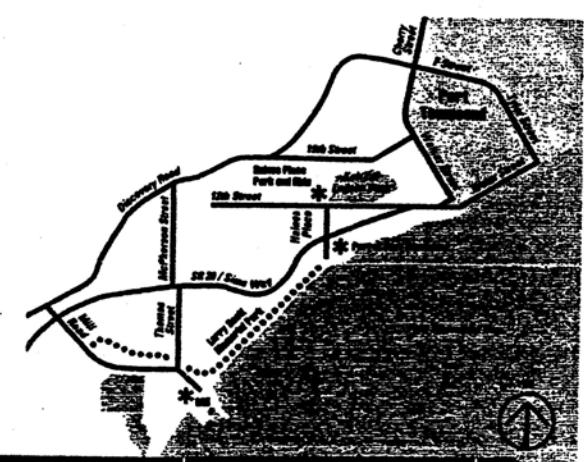
Larry Scott Memorial Park

Project Number STPE-2016 (008)

INDEX OF DRAWINGS

- L1 LAYOUT, GRADING AND RESTORATION PLAN
L2 LAYOUT, GRADING AND RESTORATION PLAN
L3 LAYOUT, GRADING AND RESTORATION PLAN
L4 LAYOUT, GRADING AND RESTORATION PLAN
L5 LAYOUT, GRADING AND RESTORATION PLAN
L6 LAYOUT, GRADING AND RESTORATION PLAN
L7 LAYOUT, GRADING AND RESTORATION PLAN
L8 LAYOUT, GRADING AND RESTORATION PLAN
L9 LAYOUT, GRADING AND RESTORATION PLAN
L10
L11 DETAILS

Vicinity Map

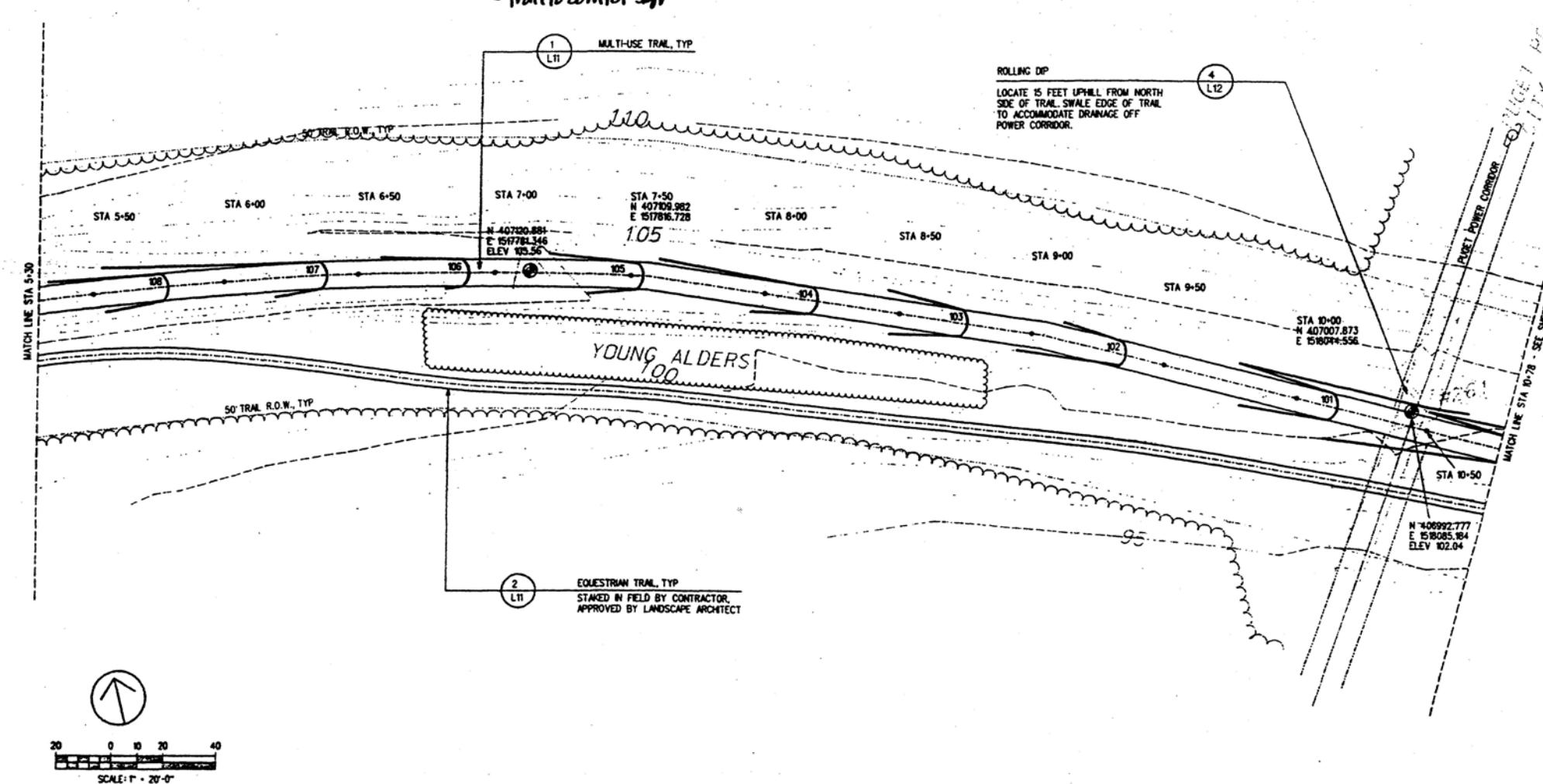
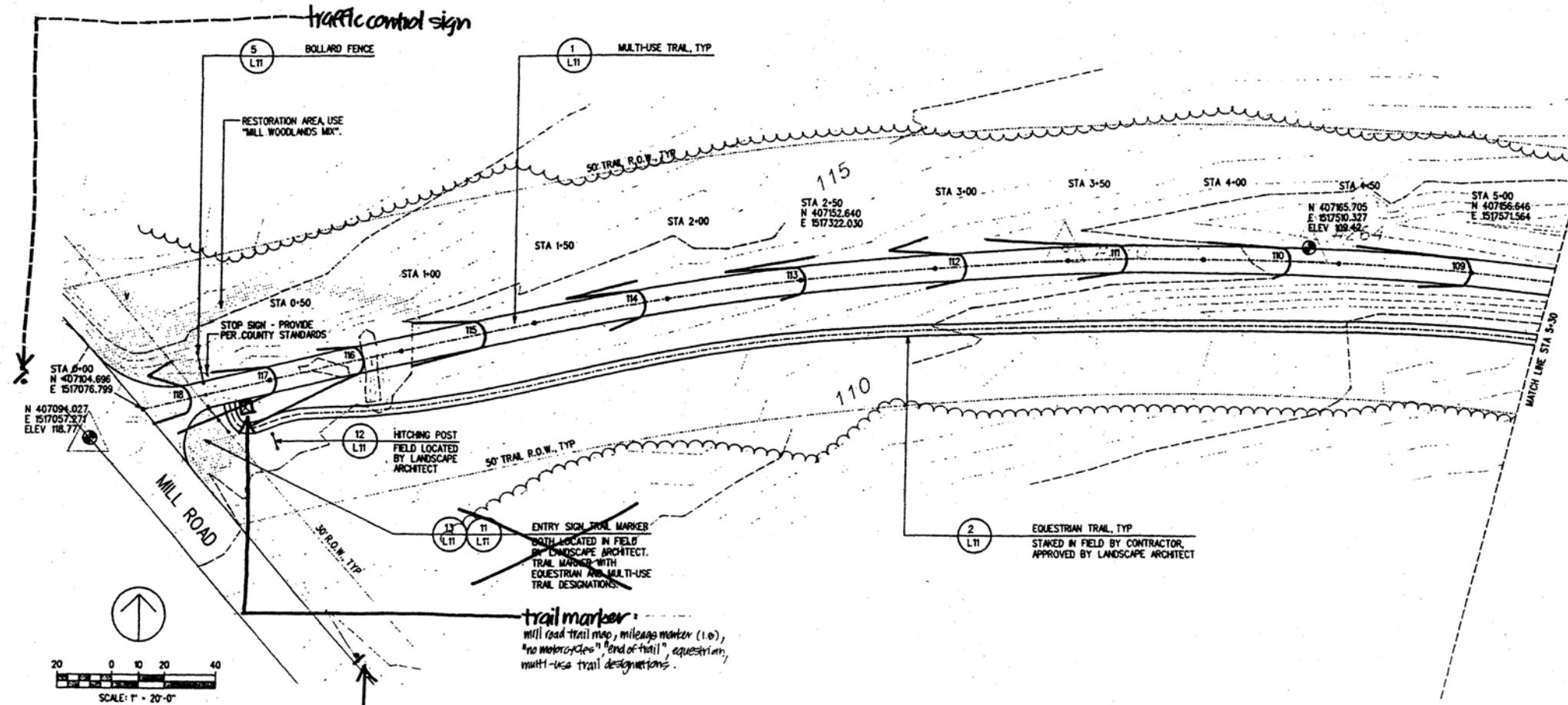


Nakano-Dennis Landscape Architecture
CH2M Hill Civil Engineering Clark Land Office Surveying Linda Feltner Graphic Design

NAKANO

DENNIS

LANDSCAPE ARCHITECTS
300 EAST PINE STREET
SEATTLE, WASHINGTON 98122
P 206/232-9392 F 206/232-9640

RENEWAL
CERTIFICATE NO. 335**SCHEDULE**

	RESTORATION AREAS, SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.
	SHORELINE RERAP REPAIR AREA
	WETLAND
	PROPOSED CONTOUR
	EXISTING CONTOURS
	CENTERLINE OF TRAIL
	RIGHT OF WAY, SEE PLAN
	SILT FILTRATION FENCE
	EXISTING TREE EDGE
	STATION POINT OF MULTI-USE TRAIL AND ASSOCIATED COORDINATES AT 250' INTERVALS.
	SURVEYED BENCHMARK AND ASSOCIATED COORDINATES

NOTES

1. CENTERLINE OF TRAIL SHALL BE SURVEYED ON SITE BY CONTRACTOR. SURVEY TO INCLUDE STATION POINTS AND ELEVATIONS WITH PINS AT 50' INTERVALS. MATCHING STATION POINTS SHOWN ON PLAN. LANDSCAPE ARCHITECT TO APPROVE CENTERLINE OF TRAIL BEFORE CONSTRUCTION BEGINS.
2. BASE INFORMATION HAS BEEN COMPILED FROM A COMBINATION OF AERIAL PHOTOGRAPHS AND FIELD SURVEYS. SEE NOTES ON PLANS FOR CHANGES IN SURVEY SOURCES ALONG TRAIL LENGTH. CONTRACTOR SHALL VERIFY SURVEY BEFORE CONSTRUCTION BEGINS.
3. CONTRACTOR SHALL SMOOTH OUT BERMS AND SHOULDERS AND CLEAR SWALES ALONG TRAIL TO FACILITATE DRAINAGE, TYP.
4. CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS WITHIN 100' TRAIL RIGHT OF WAY TO A REGULATED APPROVED FACILITY OFF SITE.
5. CONTRACTOR SHALL PRESERVE ALL EXISTING VEGETATION ALONG TRAIL, TYP. SEE SPECIFICATIONS FOR VEGETATION PROTECTION.
6. ALL EXISTING DEPRESSIONS ALONG TRAIL ARE TO BE FILLED, TYP.
7. FROM STATION POINT 50-00 TO 89-00 RESTORE EXPOSED/ERODING/BARE SOIL AREA ALONG RERAP SLOPE USING CLASS A ROCK AND RELOCATING EXISTING CONCRETE RERAP TO REINFORCE AND ARMOR SHORELINE BANK. EXTENT OF RESTORATION TO BE DETERMINED ON SITE WITH JEFFERSON COUNTY AT PRE-BID WALK THRU KEY RESTORATION AREAS ARE NOTED WITH SYMBOL - SEE SCHEDULE REFER TO SPECIFICATIONS.
8. NO CONSTRUCTION ACCESS INTO ANY PORTION OF THE DESIGNATED WETLAND OR WETLAND BUFFER.
9. CONTRACTOR SHALL SEED ALL DISTURBED AREAS IN RULW. SEE SPECIFICATIONS.
10. CLARK LAND OFFICE SURVEY HORIZONTAL DATUM: WASHINGTON COORDINATE SYSTEM OF 1927, NORTH ZONE BASED ON US COAST AND GEODETIC SURVEY STATION "PORT TOWNSEND NORTHEAST BASE".
11. CLARK LAND OFFICE VERTICAL DATUM: MEAN LOWER LOW WATER MARK TIDAL DATUM TO OBTAIN ELEVATIONS BASED ON NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29). SUBTRACT 4.72 FEET FROM THE MILL ELEVATIONS SHOWN HEREON. THE WATER LINE SHOWN HEREON IS DRAWN AT THE APPROXIMATE MEAN HIGHER HIGH WATER GARNET LINE.
12. TIDAL WATERS: MEAN HIGH-7.8, MEAN HIGHER HIGH-8.6, MEAN LOWER LOW WATER-0.0.
13. STRAW BALE DETAIL 1 ON SHEET L1 TO OCCUR EVERY 150' IN SIDE SWALE ON UPHILL SIDE OF TRAIL.
14. REFERENCE DETAIL B ON SHEET L1 TO PROVIDE THIS DETAIL ALONG TRAIL ONLY WHEN SURFACE DRAINAGE FLOW CANNOT BE ACCOMMODATED BY SHEET FLOW OR SWALE.
15. TRAIL EASEMENT SURVEY AT PORT (SHEETS L9, L10) DEVELOPED BY JEFFERSON COUNTY PUBLIC WORKS DEPARTMENT LANE 1997.

Larry Scott Memorial Park

PORT TOWNSEND, WASHINGTON

CLIENT

JEFFERSON COUNTY PUBLIC WORKS
P.O. BOX 1270
PORT TOWNSEND, WASHINGTON 98373PAGE
CONSTRUCTION DOCUMENTS
3 JULY 1997

APPROVALS

DESIGNED BY: CHECKED BY:
DRAWN BY: APPROVED BY:

REVISES

NO. DATE DESCRIPTION

SHEET TITLE
LAYOUT, GRADING
AND RESTORATION
PLANSHEET NUMBER
L1

SCHEDULE

RESTORATION AREAS, SEE PLAN FOR SEED
MIX AND SPECIFICATIONS FOR PREPARATION.

SHORELINE RIRRAP REPAIR AREA

WETLAND

— 1 —

59

EXISTING CONTOURS

CENTERLINE OF TRAIL

www.sciencedirect.com

— SILT FILTRATION FENCE

 EXISTING TREE EDGE

2+50
07152.640
17322.030 STATION POINT OF MULTI-USE TRAIL
AND ASSOCIATED COORDINATES AT
250' INTERVALS.

17638.253 SURVEYED BENCHMARK AND
22793.704 ASSOCIATED COORDINATES
16 56

10

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CONTRACTOR SHALL SMOOTH OUT BERM'S AND SHOULDERS AND CLEAR SWALES ALONG TRAIL TO FACILITATE DRAINAGE, TYP.

CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS WITHIN 100' TRAIL RIGHT OF WAY TO A REGULATED, APPROVED FACILITY OFF SITE.

CONTRACTOR SHALL PRESERVE ALL EXISTING VEGETATION ALONG TRAIL, TYP. SEE SPECIFICATIONS FOR VEGETATION PROTECTION.

ALL EXISTING DEPRESSIONS ALONG TRAIL ARE TO BE FILLED, TYP.

FROM STATION POINT 50-00 TO 89-00 RESTORE EXPANDED/ERODING/BARE SOIL AREAS ALONG RIPRAP SLOPE USING CLASS I ROCK AND RELOCATING EXISTING CONCRETE RIPRAP TO REINFORCE AND ARMOR SHORELINE BANK. EXTENT OF RESTORATION TO BE DETERMINED ON SITE WITH JEFFERSON COUNTY AT PRE-BID WALK THROUGH. KEY RESTORATION AREAS ARE NOTED WITH SYMBOL - SEE SCHEDULE. REFER TO SPECIFICATIONS.

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TIDAL WATERS: MEAN HIGH+7.8, MEAN HIGHER HIGH+8.6, MEAN LOWER LOW WATER-0.0.

STRAW BALE DETAIL 1 ON SHEET LT TO OCCUR EVERY 150' IN SIDE SWALE ON UPHILL SIDE OF TRAIL. REFERENCE DETAIL 8 ON SHEET LT PROVIDE THIS DETAIL ALONG TRAIL ONLY WHEN SURFACE DRAINAGE FLOW CANNOT BE ACCOMMODATED BY SHEET FLOW OR SWALE.

TRAIL EASEMENT SURVEY AT PORT (SHEETS L9, L10; DEVELOPED BY JEFFERSON COUNTY PUBLIC WORKS DEPARTMENT, JUNE 2024).



REGISTERED
LANDSCAPE ARCHITECT

KENONI HAKANO
CERTIFICATE NO. J35

CH2M HILL
CIVIL ENGINEERING
617 TYLER STREET
MINNEAPOLIS, MINNESOTA 55402
65-2413 F 360/379-5177

ARK LAND OFFICE
SURVEYORS
P.O. BOX 2199
JUL, WASHINGTON 98582
509-756-2161 F 509-756-5510

LINDA FELTNER
GRAPHIC DESIGN
P.O. BOX 4251
TITLE, WASHINGTON 98104
TEL. 206-582-1222

PROJECT TITLE

Larry
Scott
Memorial
Park



**JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS**

www.ijerpi.org

INSTRUCTION DOCUMENTS
JULY 1997

SEARCHED BB.DW CHECKED BB
INDEXED BB SERIALIZED BB

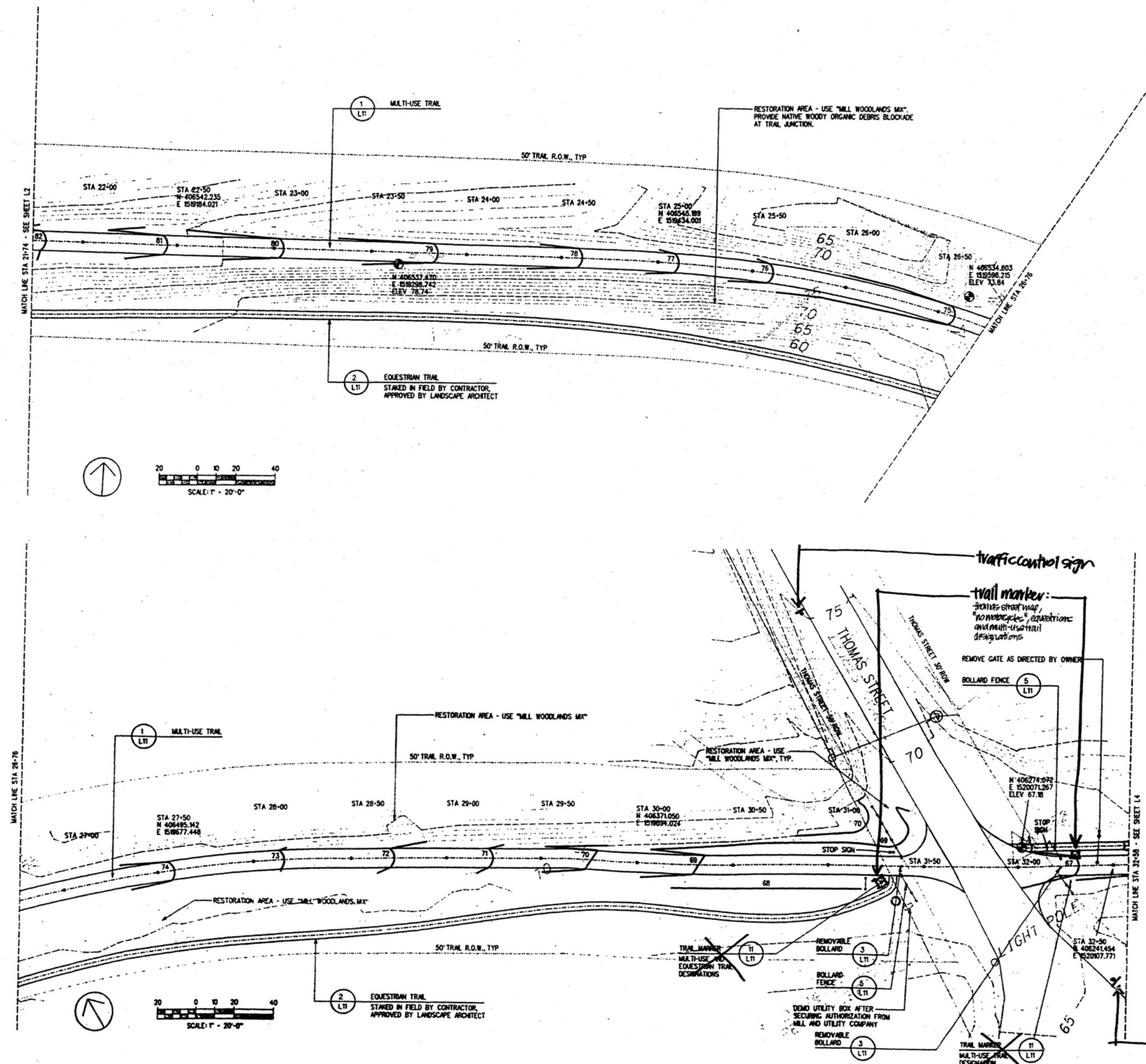
ITEMS

— 1 —

ET TITLE
YOUT, GRADING
D RESTORATION
AN

IT MANNER

3



NAKANO
DENNIS
LANDSCAPE ARCHITECTS
300 EAST PIKE STREET
SEATTLE, WASHINGTON 98102
P 206/292-5392 F 206/292-9640



KENNEWICK
CERTIFICATE NO 335

SCHEDULE

	RESTORATION AREAS, SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.
	SHORELINE RIPRAP REPAIR AREA
	WETLAND
	PROPOSED CONTOUR
	EXISTING CONTOURS
	CENTERLINE OF TRAIL
	RIGHT OF WAY, SEE PLAN
	SILT FILTRATION FENCE
	EXISTING TREE EDGE
	STATION POINT OF MULTI-USE TRAIL AND ASSOCIATED COORDINATES AT 250' INTERVALS.
	N 407638.253 E 1522793.704 SURVEYED BENCHMARK AND E 1517322.030 ASSOCIATED COORDINATES ELEV 16.56

CHIN HILL
CIVIL ENGINEERING
607 TYLER STREET
PORT TOWNSEND, WASHINGTON 98368
P 360/385-2413 F 360/379-5177

CLARK LAND OFFICE
SURVEYORS
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SEACOM, WASHINGTON 98382
P 360/681-2191 F 360/683-5310

LINDA FELTNER
CIVIL DESIGN
P.O. BOX 2199
SEACOM, WASHINGTON 98384
P 206/343-5338 F 206/624-2606

PROJECT TITLE

Larry Scott
Memorial Park


PORT TOWNSEND, WASHINGTON

CLIENT
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1274
PORT TOWNSEND, WASHINGTON 98373

PAGE
CONSTRUCTION DOCUMENTS
3 JULY 1997

APPROVALS
DESIGNED BY: CHECKED BY
DRAWN BY: APPROVED BY

REVISING
NO. DATE DESCRIPTION

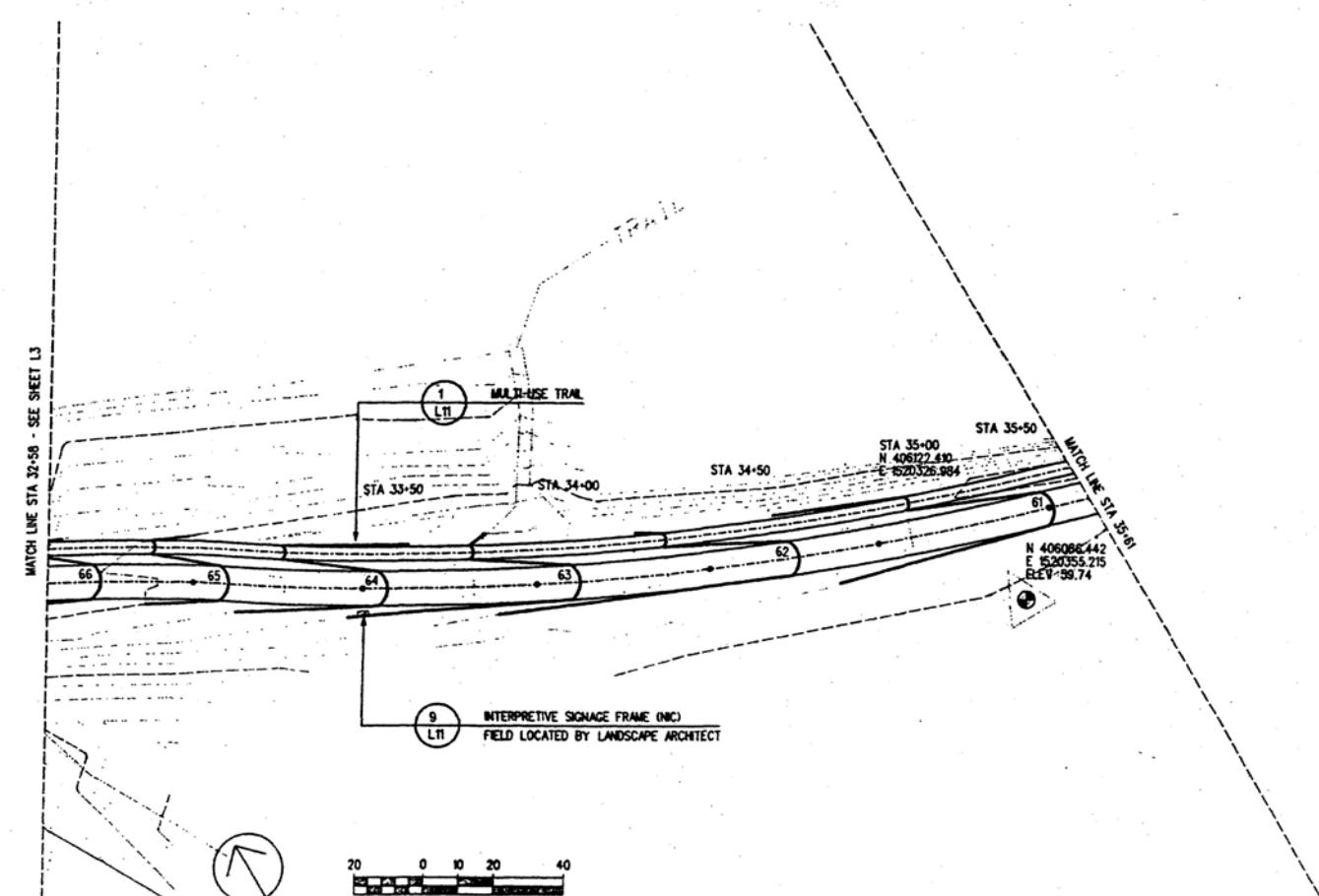
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LAYOUT, GRADING
AND RESTORATION
PLAN

SHEET NUMBER

L4

MATCH LINE STA 32-58 - SEE SHEET L3

MATCH LINE STA 35-61

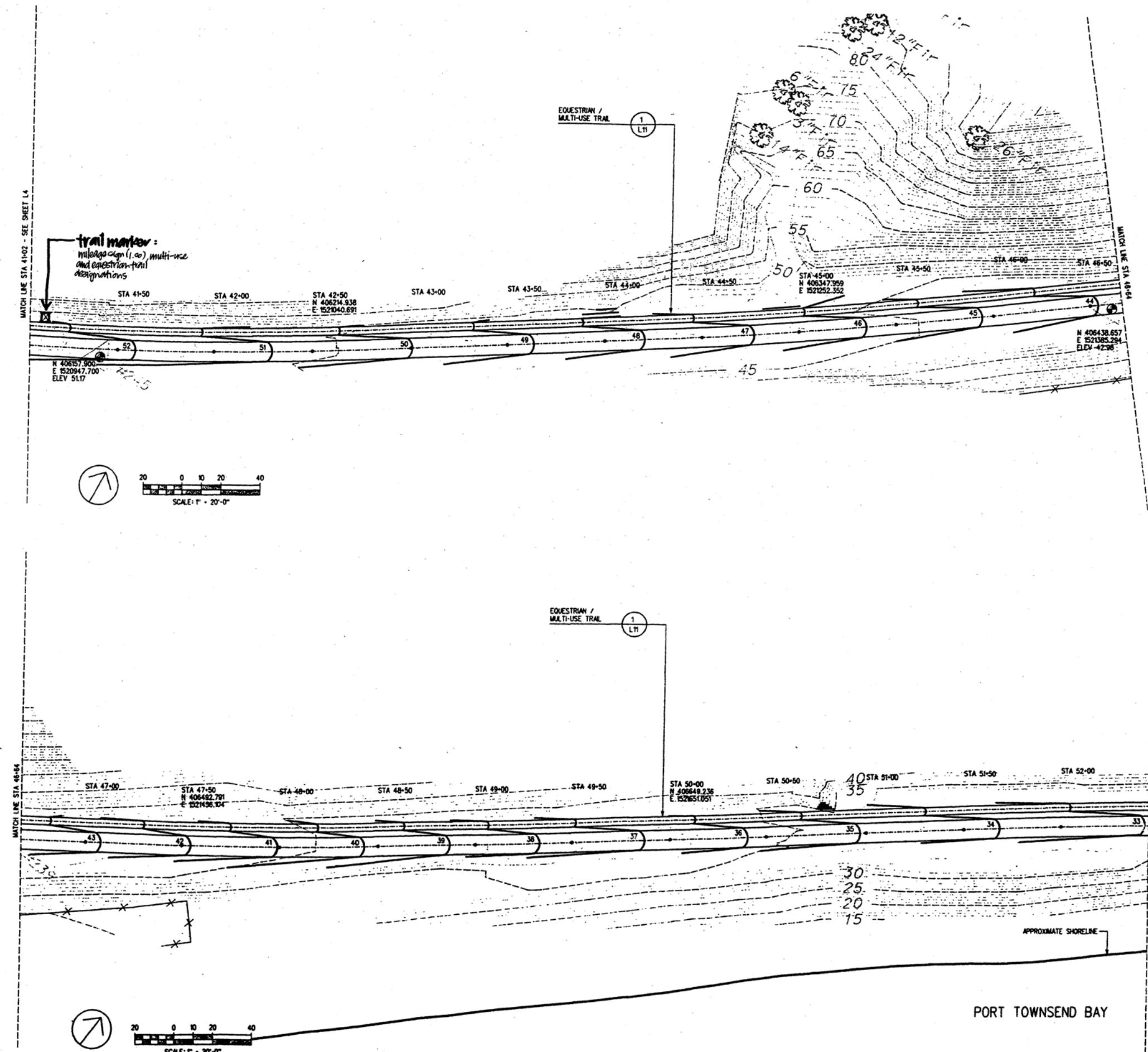


NAKANO
DENNIS
LANDSCAPE ARCHITECTS
300 EAST PINE STREET
SEATTLE, WASHINGTON 98122
P 206/292-9392 F 206/292-9540



STATE OF
WASHINGTON
REGISTERED
LANDSCAPE ARCHITECT

ENRICHING
CERTIFICATE NO. 315



SCHEDULE

- RESTORATION AREAS SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.
- SHORELINE RIPRAP REPAIR AREA
- WETLAND
- PROPOSED CONTOUR
- EXISTING CONTOURS
- CENTERLINE OF TRAIL
- RIGHT OF WAY, SEE PLAN
- SILT FILTRATION FENCE
- EXISTING TREE EDGE
- STA 2-50 N 40752.640 E 1517322.030 STATION POINT OF MULTI-USE TRAIL AND ASSOCIATED COORDINATES AT 25' INTERVALS.
- N 407638.253 E 1522703.704 SURVEYED BENCHMARK AND ASSOCIATED COORDINATES

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CH2M HILL
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877 TUDOR STREET
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P 360/681-2161 F 360/683-5310

LINDA FELTNER
GRAPHIC DESIGN
P.O. BOX 4251
SEATTLE, WASHINGTON 98104
P 206/343-5338 F 206/624-2606

PROJECT TITLE
Larry Scott Memorial Park

PORT TOWNSEND, WASHINGTON

CLIENT
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1770
PORT TOWNSEND, WASHINGTON 98368

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CONSTRUCTION DOCUMENTS
3 JULY 1997

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RECORDED: DRAWN: APPROVED:

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SHEET TITLE
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PLAN

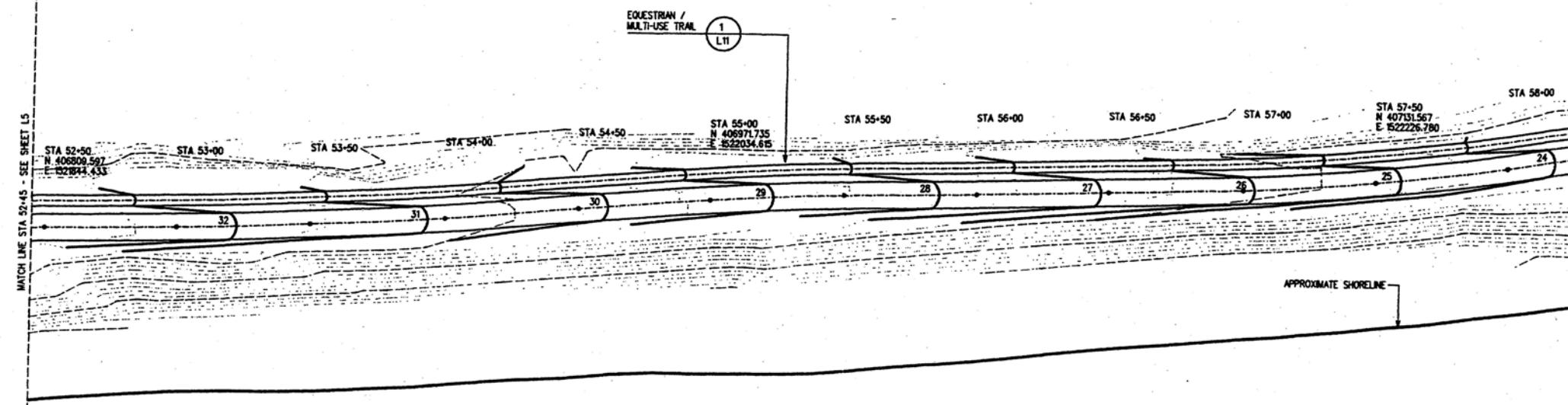
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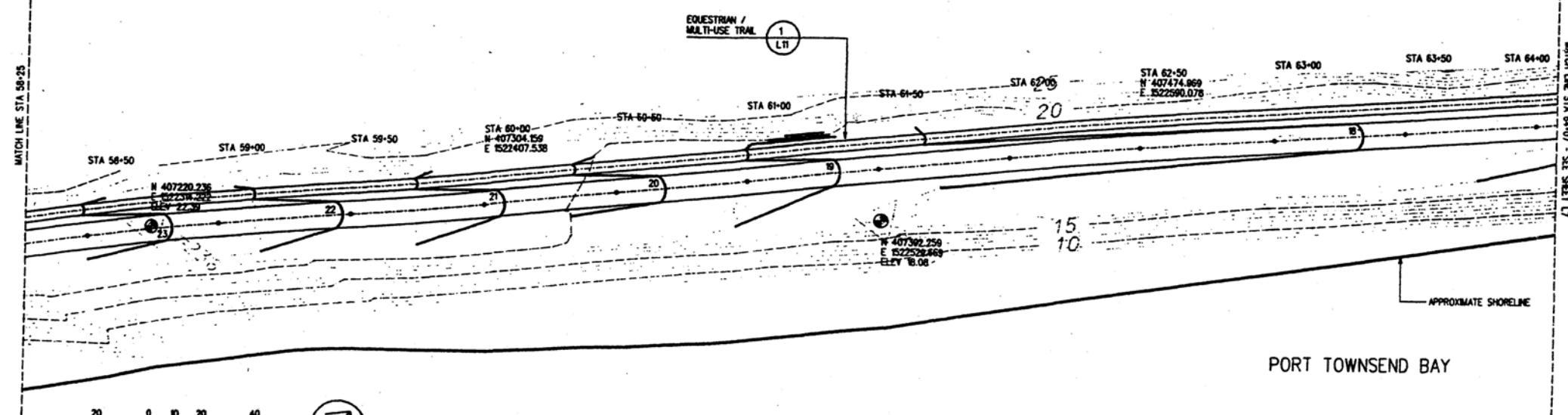
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SEATTLE, WASHINGTON 98122
P 206/292-6382 F 206/292-9640

STATE OF
REGISTERED
LANDSCAPE ARCHITECT
JERON NAKANO
CERTIFICATE NO. 335



PORT TOWNSEND BAY



PORT TOWNSEND BAY

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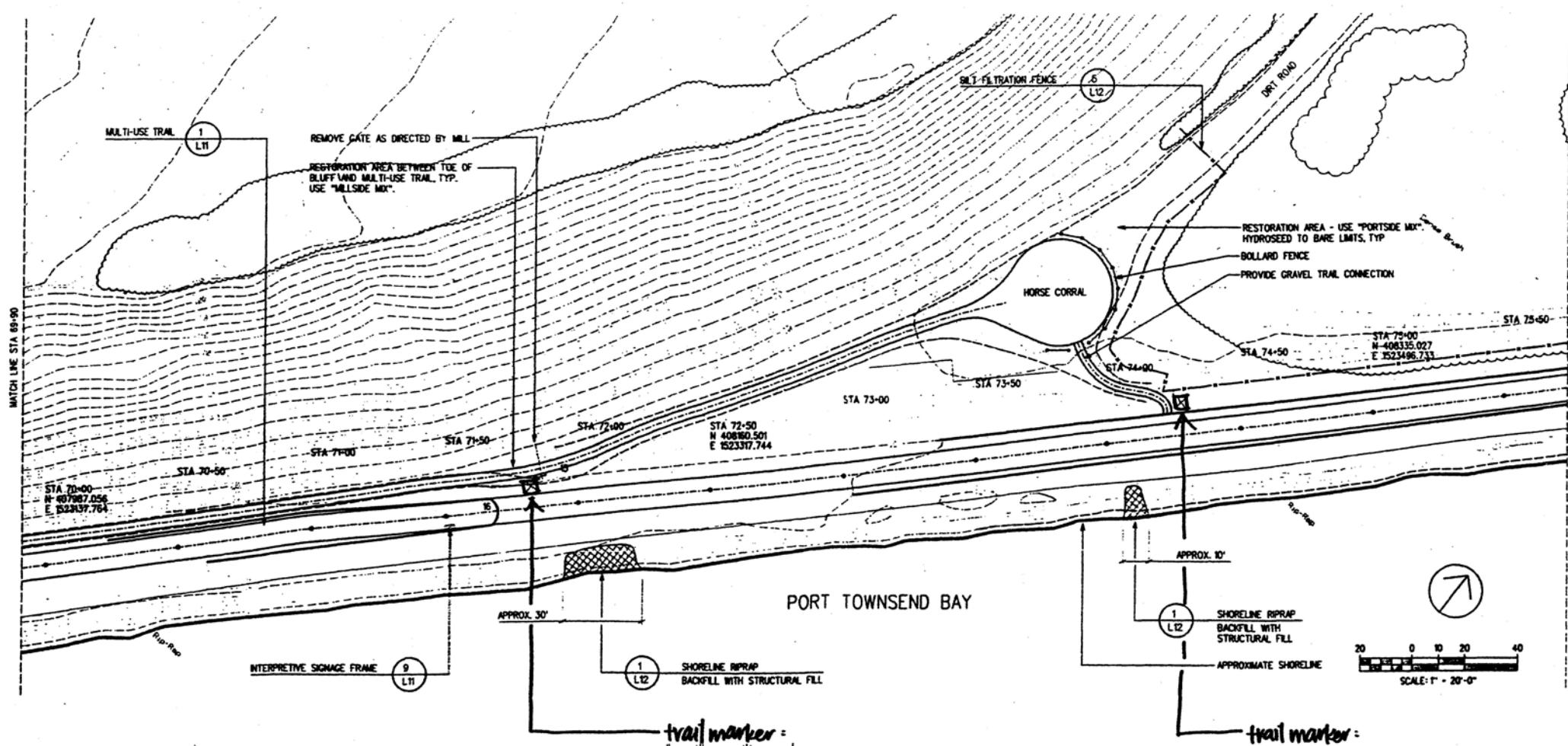
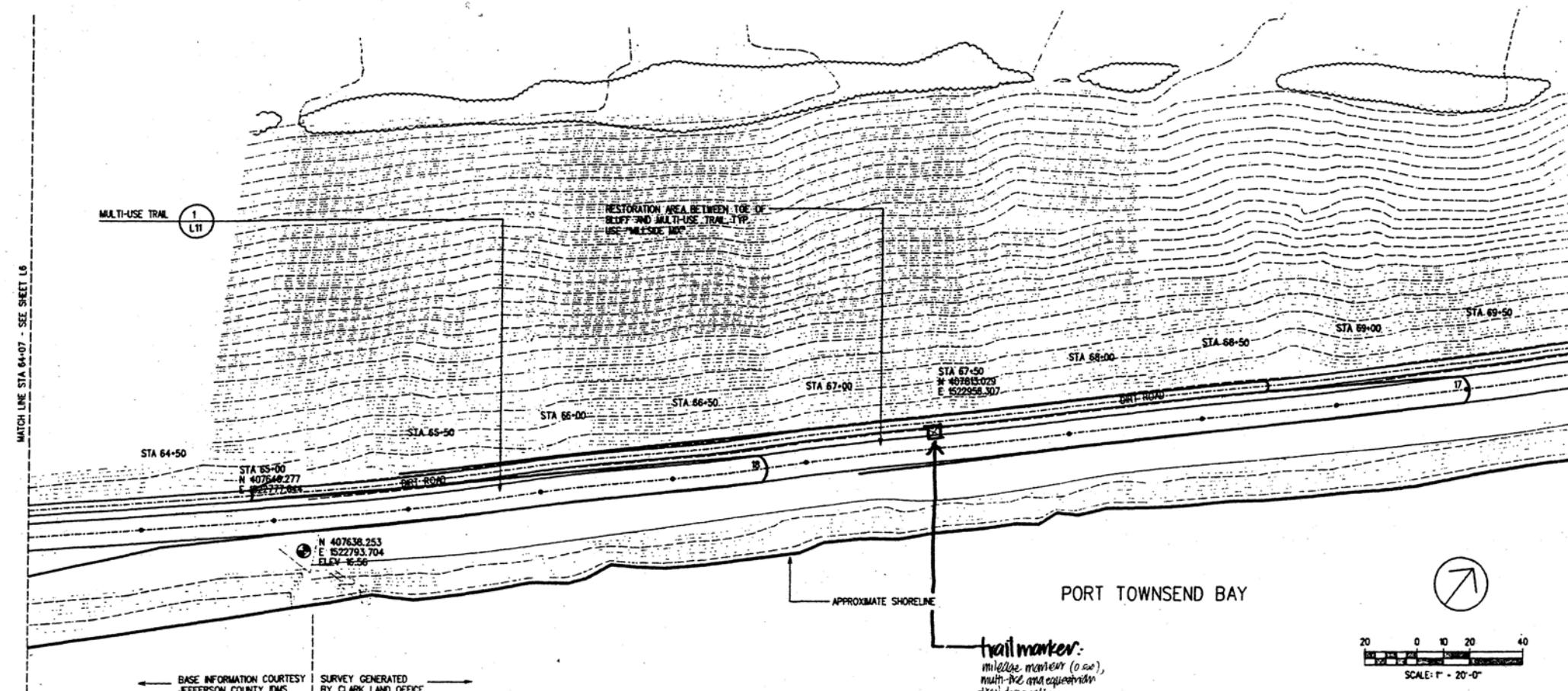
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LAYOUT, GRADING
AND RESTORATION
PLAN

SHEET NUMBER
L6



SCHEDULE

RESTORATION AREAS SEE PLAN FOR SEED
 MIX AND SPECIFICATIONS FOR PREPARATION.

 SHORELINE RIPRAP REPAIR AREA

 WETLAND

 PROPOSED CONTOUR
 61 60 59

 EXISTING CONTOURS
 61 60 59

CENTERLINE OF TRAIL

RIGHT OF WAY, SEE PLAN

SILT FILTRATION FENCE

EXISTING TREE EDGE

STA 2-50
 N 407526.640
 E 1517322.030

STATION POINT OF MULTI-USE TRAIL
 AND ASSOCIATED COORDINATES AT
 250' INTERVALS.

•

N 407638.253
 E 1522793.704
 ELEV. 16.56

SURVEYED BENCHMARK AND
 ASSOCIATED COORDINATES

CH2M HILL
CIVIL ENGINEERING
617 TYLER STREET
PORT TOWNSEND, WASHINGTON 98368
P. 360-368-5411 FAX 360-368-5407

P.O. BOX 2199
SEQUIM, WASHINGTON 98382
P 360/681-2161 F 360/683-5310

The logo for Larry Scott Memorial Park is centered on a white background. It features the park's name in a bold, black, sans-serif font. Below the text is a graphic element consisting of two stylized pine trees on the left and a wavy line representing water or a path on the right.

CREDIT
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1270

PHASE
CONSTRUCTION DOCUMENTS
3-11-97

APPROVALS

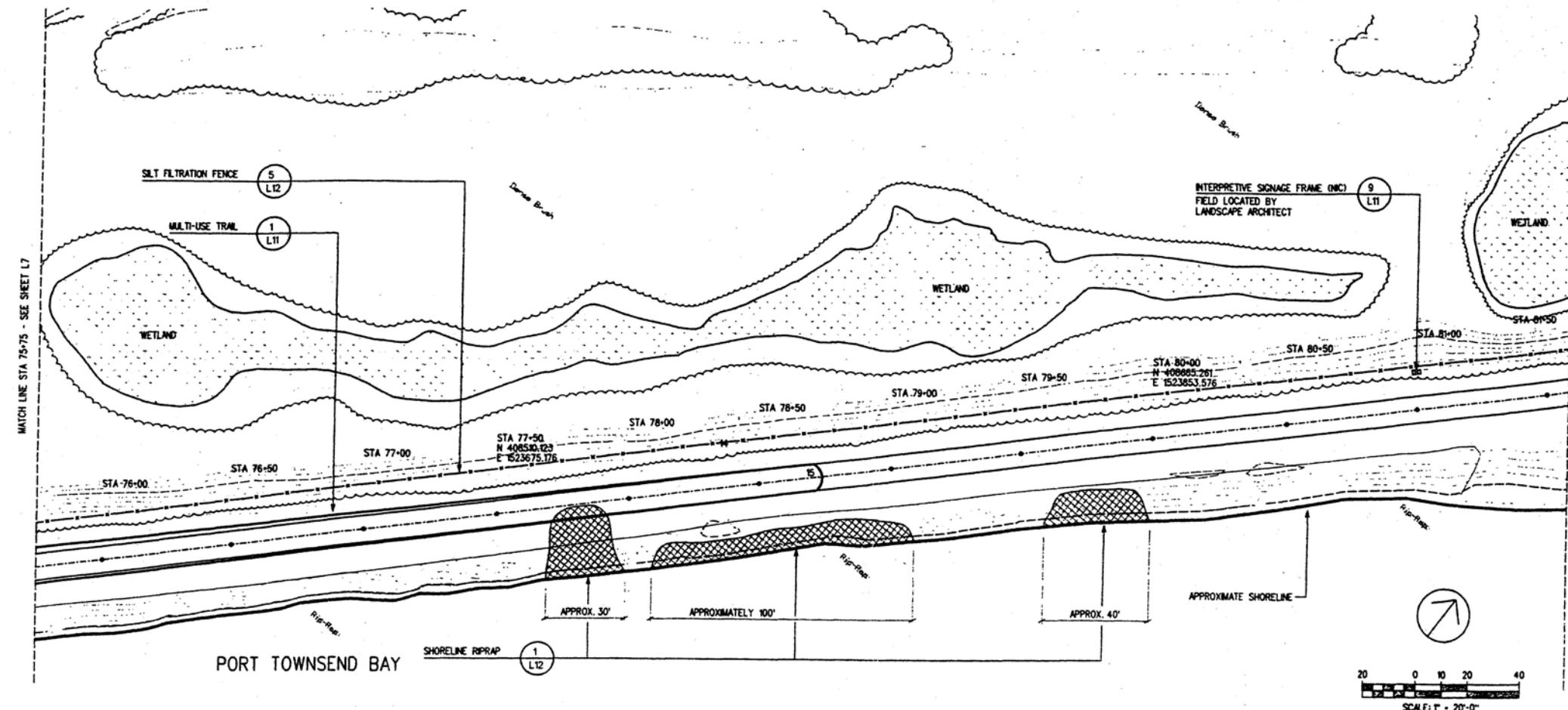
SEARCHED	SERIALIZED	INDEXED	FILED
DR-10			
APPROVED BY			
<u>REVISIONS</u>			
NO.	DATE	DESCRIPTION	

LAWRENCE BERKELEY NATIONAL LABORATORY • BERKELEY, CALIFORNIA

SHEET TITLE
**LAYOUT, GRADING
AND RESTORATION
PLAN**

~~SECRET~~ CLASSIFIED

17



SCHEDULE

RESTORATION AREAS. SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.

 SHORELINE RIPRAP REPAIR AREA

WETLAND

61 / 60 PROPOSED CONTOUR

EXISTING CONTOURS

— 30 —

SET PENTRATION FORCE

EXISTING TREE EDGE
STA 2-50 STATION POINT OF MULTI-USE TRAIL
N 407152 E 640 AND ASSOCIATED COORDINATES AT

N 407638.253 SURVEYED BENCHMARK AND
E 1522793.704 ASSOCIATED COORDINATES
ELEV. 16.55

NAKANO
◆
DENSICS

LANDSCAPE ARCHITECTS
300 EAST PINE STREET
SEATTLE, WASHINGTON 98122
P 206/292-9392 F 206/292-9640



~~RECORDED MAR 1980~~
CERTIFICATE NO. 335

CH2M HILL
CIVIL ENGINEERING
617 TYLER STREET
PORT TOWNSEND, WASHINGTON 98368
(360) 385-2111 FAX (360) 385-2275

CLARK LAND OFFICE
SURVEYORS
P.O. BOX 2199
SEDUM, WASHINGTON 98382
TELEGRAMS: DIAL E. TELEX 4200

LINDA FELTNER
GRAPHIC DESIGN
P.O. BOX 4251
SEATTLE, WASHINGTON 98104

NOTES

1. CENTERLINE OF TRAIL SHALL BE SURVEYED ON SITE BY CONTRACTOR. SURVEY TO INCLUDE STATION POINTS AND ELEVATIONS WITH PINS AT 50' INTERVALS MATCHING STATION POINTS SHOWN ON PLAN. LANDSCAPE ARCHITECT TO APPROVE CENTERLINE OF TRAIL BEFORE CONSTRUCTION BEGINS.
 2. BASE INFORMATION HAS BEEN COMPILED FROM A COMBINATION OF AERIAL PHOTOGRAPHS AND FIELD SURVEYS. SEE NOTES ON PLANS FOR CHANGES IN SURVEY SOURCES ALONG TRAIL LENGTH. CONTRACTOR SHALL FIELD VERIFY SURVEY BEFORE CONSTRUCTION BEGINS.
 3. CONTRACTOR SHALL SMOOTH OUT BERMS AND SHOULDERS AND CLEAR SWALES ALONG TRAIL TO FACILITATE DRAINAGE, TYP.
 4. CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS WITHIN 100' TRAIL RIGHT OF WAY TO A REGULATED, APPROVED FACILITY OFF SITE.
 5. CONTRACTOR SHALL PRESERVE ALL EXISTING VEGETATION ALONG TRAIL, TYP. SEE SPECIFICATIONS FOR VEGETATION PROTECTION.
 6. ALL EXISTING DEPRESSIONS ALONG TRAIL ARE TO BE FILLED, TYP.
 7. FROM STATION POINT 50+00 TO 89+00 RESTORE EXPOSED/ERODING/BARE SOL AREAS ALONG REPAIR SLOPE. USING CLASS A ROCK AND RELOCATING EXISTING CONCRETE REPAIR TO REINFORCE AND ARMOR SHORELINE BANK. EXTENT OF RESTORATION TO BE DETERMINED ON SITE WITH JEFFERSON COUNTY AT PRE-BID WALK THRU. KEY RESTORATION AREAS ARE NOTED WITH SYMBOL - SEE SCHEDULE. REFER TO SPECIFICATIONS.
 8. NO CONSTRUCTION ACCESS INTO ANY PORTION OF THE DESIGNATED WETLAND OR WETLAND BUFFER.
 9. CONTRACTOR SHALL SEED ALL DISTURBED AREAS IN R.O.W. SEE SPECIFICATIONS.
 10. CLARK LAND OFFICE SURVEY HORIZONTAL DATUM: WASHINGTON COORDINATE SYSTEM OF 1927, NORTH ZONE BASED ON US COAST AND GEODETIC SURVEY STATION "PORT TOWNSEND NORTHEAST BASE".
 11. CLARK LAND OFFICE VERTICAL DATUM: MEAN LOWER LOW WATER MARK TIDAL BATH TO OBTAIN ELEVATIONS BASED ON NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29). SUBTRACT 4.72 FEET FROM THE MLLW ELEVATIONS SHOWN HEREON. THE WATER LINE SHOWN HEREON IS DRAWN AT THE APPROXIMATE MEAN HIGHER HIGH WATER (MHHW) LINE.
 12. TIDAL WATERS: MEAN HIGH-7.5, MEAN HIGHER HIGH-8.6, MEAN LOWER LOW WATER-0.0.
 13. STRAW BALE DETAIL 1 ON SHEET L1 TO OCCUR EVERY 150' IN SIDE SWALE ON UPHILL SIDE OF TRAIL.
 14. REFERENCE DETAIL 8 ON SHEET L1 TO PROVIDE THIS DETAIL ALONG TRAIL ONLY WHEN SURFACE DRAINAGE FLOW CANNOT BE ACCOMMODATED BY SHEET FLOW OR SWALE.
 15. TRAIL EASEMENT SURVEY AT PORT (SHEETS L9, L10) DEVELOPED BY JEFFERSON COUNTY PUBLIC WORKS DEPARTMENT (JUNE 1992).

PORT TOWNSEND, WASHINGTON

222

JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1270
PORT JEFFERSON, NEW YORK 11776

PHASE
CONSTRUCTION DOCUMENTS
7-25-1997

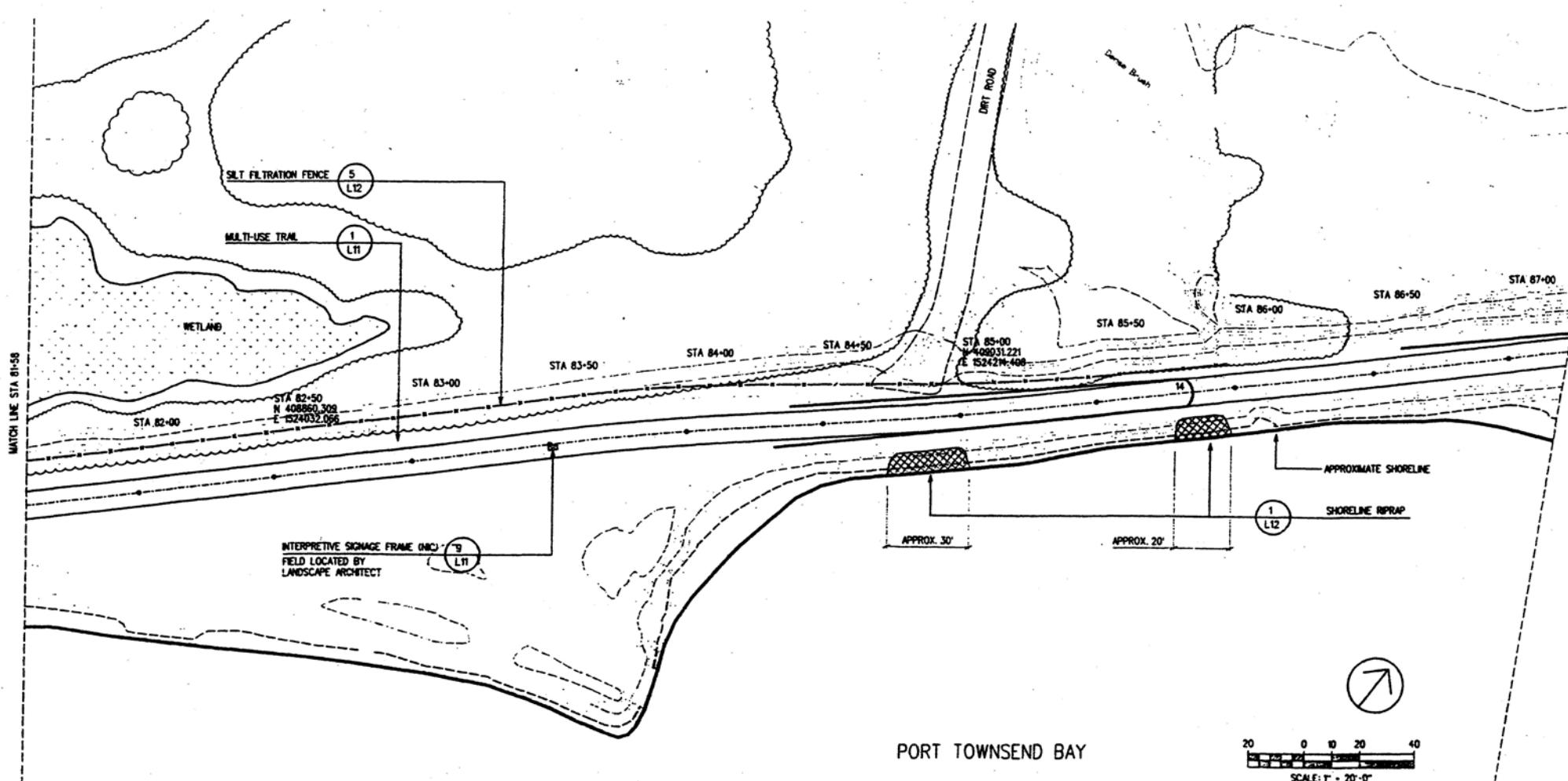
ANSWERS

DESIGNED BY: DD
DRAWN BY: EM APPROVED BY: DD

NO. DATE ORIGINATOR

SHEET TITLE
**LAYOUT, GRADING
AND RESTORATION
PLAN**

©2003 Warner



NAKANO
DENNIS

LANDSCAPE ARCHITECTS
300 EAST PINE STREET
SEATTLE, WASHINGTON 98122
P 206/222-6392 F 206/222-9640



KENDRICK AND CO. INC.
CERTIFICATE NO. 335

SCHEDULE

	RESTORATION AREAS, SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.
	SHORELINE RIPRAP REPAIR AREA
	WETLAND
	PROPOSED CONTOUR
	EXISTING CONTOURS
	CENTERLINE OF TRAIL
	RIGHT OF WAY, SEE PLAN
	SILT FILTRATION FENCE
	EXISTING TREE EDGE
	STATION POINT OF MULTI-USE TRAIL AND ASSOCIATED COORDINATES AT 250' INTERVALS.
	407638.253 SURVEYED BENCHMARK AND ELEV 16.56 ASSOCIATED COORDINATES

NOTES

1. CENTERLINE OF TRAIL SHALL BE SURVEYED ON SITE BY CONTRACTOR. SURVEY TO INCLUDE STATION POINTS AND ELEVATIONS WITH PIN'S AT 50' INTERVALS. MATCHING STATION POINTS SHOWN ON PLAN. LANDSCAPE ARCHITECT TO APPROVE CENTERLINE OF TRAIL BEFORE CONSTRUCTION BEGINS.
2. BASE INFORMATION HAS BEEN COMPILED FROM A COMBINATION OF AERIAL PHOTOGRAPHS AND FIELD SURVEYS. SEE NOTES ON PLANS FOR CHANGES IN SURVEY SOURCES ALONG TRAIL LENGTH. CONTRACTOR SHALL FELTNER SURVEY BEFORE CONSTRUCTION BEGINS.
3. CONTRACTOR SHALL SMOOTH OUT BERMS AND SHOULDERS AND CLEAR SWALES ALONG TRAIL TO FACILITATE DRAINAGE TYP.
4. CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS WITHIN 100' TRAIL RIGHT OF WAY TO A REGULATED, APPROVED FACILITY OFF SITE.
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6. ALL EXISTING DEPRESSIONS ALONG TRAIL ARE TO BE FILLED, TYP.
7. FROM STATION POINT 50-00 TO 89-00 RESTORE EXPOSED/ERODING/BARE SOIL AREAS ALONG RIPRAP SLOPE USING CLASS A ROCK AND RELOCATING EXISTING CONCRETE RIPRAP TO REINFORCE AND ARMOR SHORELINE BANK. EXTENT OF RESTORATION TO BE DETERMINED ON SITE WITH JEFFERSON COUNTY AT PRE-BID WALK THROUGH. KEY RESTORATION AREAS ARE NOTED WITH SYMBOL - SEE SCHEDULE. REFER TO SPECIFICATIONS.
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11. CLARK LAND OFFICE VERTICAL DATUM: MEAN LOWER LOW WATER MARK TIDAL DATUM. TO OBTAIN ELEVATIONS BASED ON NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29), SUBTRACT 4.72 FEET FROM THE MLLW ELEVATIONS SHOWN HEREON. THE WATER LINE SHOWN HEREON IS DRAWN AT THE APPROXIMATE MEAN HIGHER HIGH WATER (MHHW) LINE.
12. TIDAL WATERS: MEAN HIGH-7.8, MEAN HIGHER HIGH-8.6, MEAN LOWER LOW WATER-0.0.
13. STREAM BAFFLE DETAIL 1 ON SHEET L1 TO OCCUR EVERY 150' IN SIDE SWALE ON UPHILL SIDE OF TRAIL.
14. REFERENCE DETAIL 8 ON SHEET L1 TO PROVIDE THIS DETAIL ALONG TRAIL ONLY WHEN SURFACE DRAINAGE FLOW CANNOT BE ACCOMMODATED BY SHEET FLOW OR SWALE.
15. TRAIL EASEMENT SURVEY AT PORT (SHEETS L9, L10) DEVELOPED BY JEFFERSON COUNTY PUBLIC WORKS DEPARTMENT (JUNE 1997).

CHW HILL
CIVIL ENGINEERING
617 TYLER STREET
PORT TOWNSEND, WASHINGTON 98368
P 360/365-2413 F 360/379-5177

CLARK LAND OFFICE
SURVEYORS
P.O. BOX 4251
SEACO, WASHINGTON 98382
P 360/661-2181 F 360/661-5310

LINDA FELTMER
GRAPHIC DESIGN
P.O. BOX 4251
SEACO, WASHINGTON 98382
P 360/343-5338 F 360/624-2606

PROJECT TITLE

Larry Scott
Memorial Park

CLIENT
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1270
PORT TOWNSEND, WASHINGTON 98368

PAGE
CONSTRUCTION DOCUMENTS
3 JULY 1997

APPROVALS

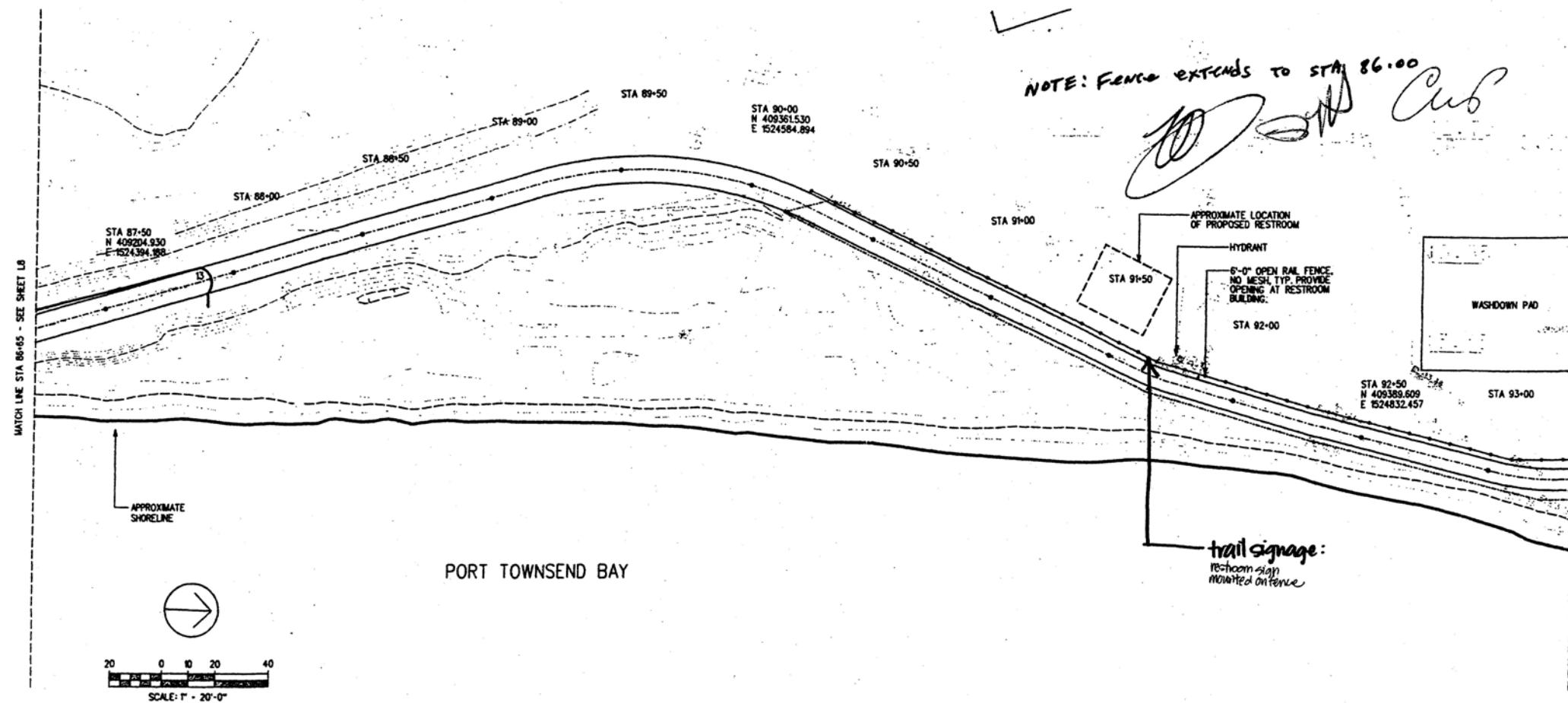
DESIGNED BY:
DRAWN BY:
APPROVED BY:

REVISIONS

NO. DATE DESCRIPTION

SHEET TITLE
LAYOUT, GRADING
AND RESTORATION
PLAN

SHEET NUMBER



SCHEDULE

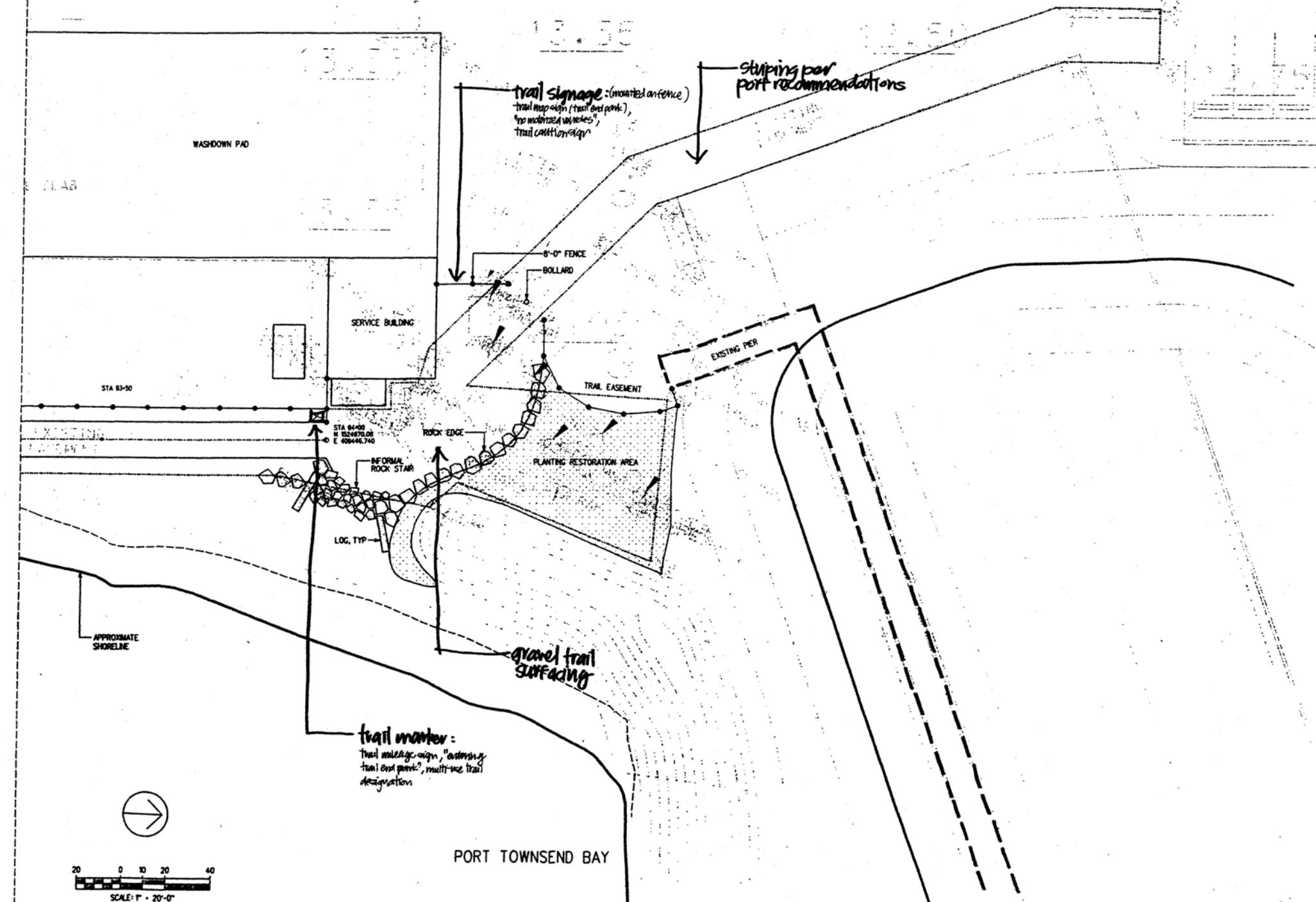
	RESTORATION AREAS, SEE PLAN FOR SEED MIX AND SPECIFICATIONS FOR PREPARATION.
	SHORELINE RIPRAP REPAIR AREA
	WETLAND
	PROPOSED CONTOUR
	EXISTING CONTOURS
	CENTERLINE OF TRAIL
	RIGHT OF WAY, SEE PLAN
	SILT FILTRATION FENCE
	EXISTING TREE EDGE
STA 2+50 N 407638.253 E 1522793.704	STATION POINT OF MULTI-USE TRAIL AND ASSOCIATED COORDINATES AT 25' INTERVALS.
N 407638.253 E 1522793.704	SURVEYED BENCHMARK AND ASSOCIATED COORDINATES

NAKANO
DENNIS
LANDSCAPE ARCHITECTS
300 EAST PINE STREET
SEATTLE, WASHINGTON 98122
P 206/292-9392 F 206/292-9640



STATE OF
WASHINGTON
REGISTERED
LANDSCAPE ARCHITECT

RENEWAL
CERTIFICATE NO 335



NOTES

1. CENTERLINE OF TRAIL SHALL BE SURVEYED ON SITE BY CONTRACTOR. SURVEY TO INCLUDE STATION POINTS AND ELEVATIONS WITH PINS AT 50' INTERVALS MATCHING STATION POINTS SHOWN ON PLAN. LANDSCAPE ARCHITECT TO APPROVE CENTERLINE OF TRAIL BEFORE CONSTRUCTION BEGINS.
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13. STRAW BALE DETAIL 1 ON SHEET L1 TO OCCUR EVERY 150' IN SIDE SWALE ON UPHILL SIDE OF TRAIL.
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CHOW HILL
CIVIL ENGINEERING
617 TYLER STREET
PORT TOWNSEND, WASHINGTON 98368
P 360/385-2413 F 360/379-5177

CLARK LAND OFFICE
SURVEYORS
P.O. BOX 299
SEQUIM, WASHINGTON 98382
P 360/681-2861 F 360/683-5310

LINDA FELTHER
GRAPHIC DESIGN
P.O. BOX 4251
SEATTLE, WASHINGTON 98104
P 206/343-5338 F 206/624-2606

PROJECT TITLE
Larry Scott Memorial Park

PORT TOWNSEND, WASHINGTON

CLIENT
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 870
PORT TOWNSEND, WASHINGTON 98368

PAGE
CONSTRUCTION DOCUMENTS
3 JULY 1997

APPROVALS
DESIGNED BY: CHECKED BY
DRAWN BY: APPROVED BY

REVISES
NO. DATE DESCRIPTION

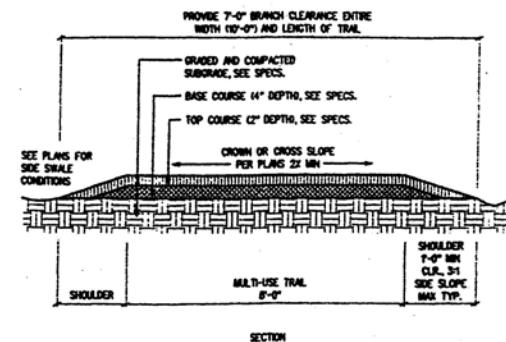
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LAYOUT, GRADING
AND RESTORATION
PLAN

SHEET NUMBER

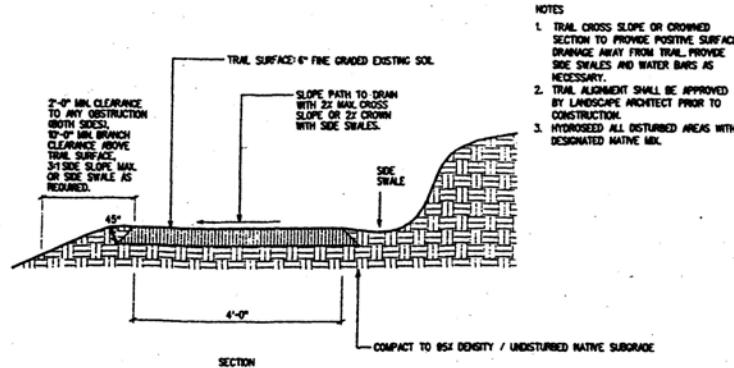
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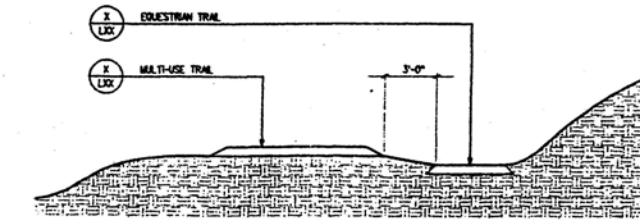
STATE OF
WASHINGTON
REGISTERED
LANDSCAPE
ARCHITECT
KENNETHAKO
CERTIFICATE NO. 335



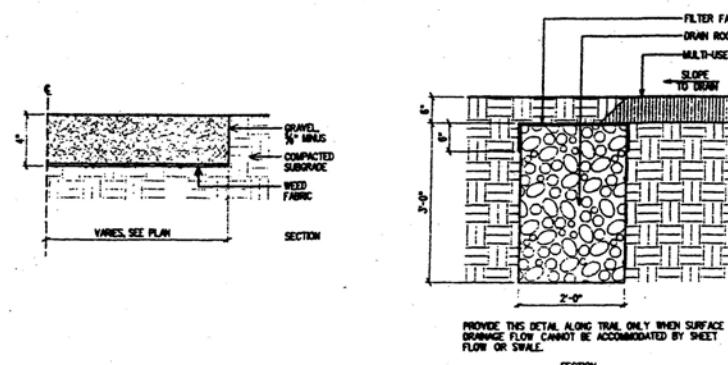
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NOT TO SCALE



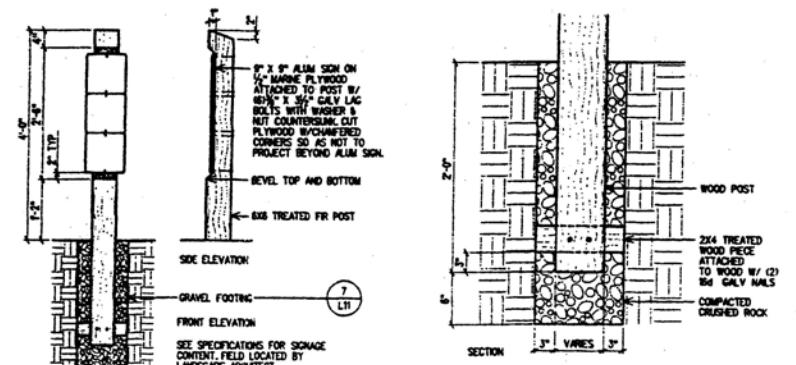
2 EQUESTRIAN TRAIL
NOT TO SCALE



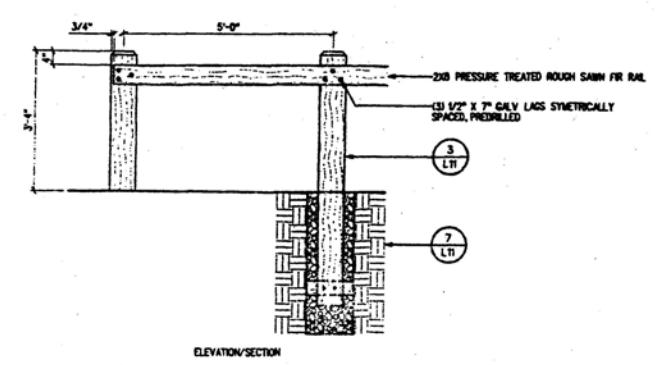
3 MULTI-USE/EQUESTRIAN TRAIL SECTION
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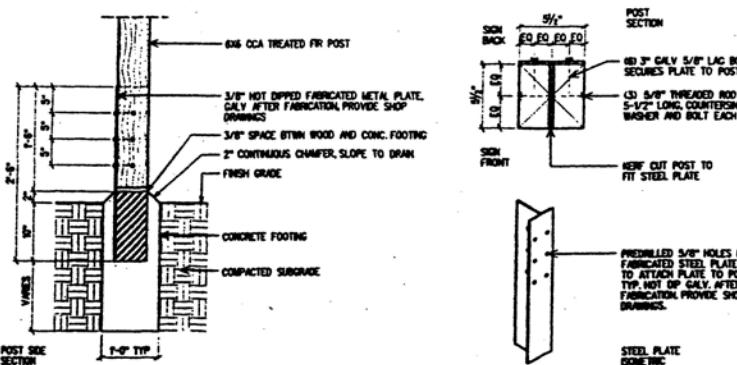
4 GRAVEL PATH
NOT TO SCALE



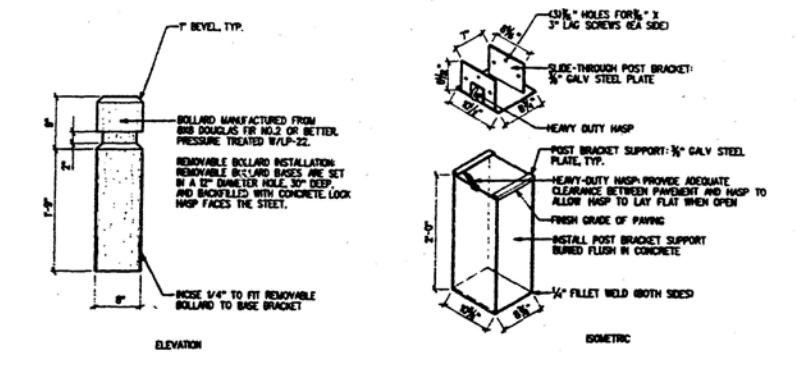
6 TRAIL MARKER
NOT TO SCALE



8 BOLLARD FENCE
NOT TO SCALE



9 WOOD POST CONCRETE FOOTING
NOT TO SCALE



10 REMOVABLE BOLLARD
NOT TO SCALE

11 REMOVABLE BOLLARD BASE
NOT TO SCALE

PROJECT TITLE
Larry Scott Memorial Park
LOGO
PORT TOWNSHIP, WASHINGTON

CUSTOMER
JEFFERSON COUNTY
DEPARTMENT OF PUBLIC WORKS
P.O. BOX 1270
PORT TOWNSHIP, WASHINGTON 98363

PAGE
CONSTRUCTION DOCUMENTS
3 JULY 1997

APPROVALS

DESIGNED BY CHECKED BY
DRAWN BY APPROVED BY

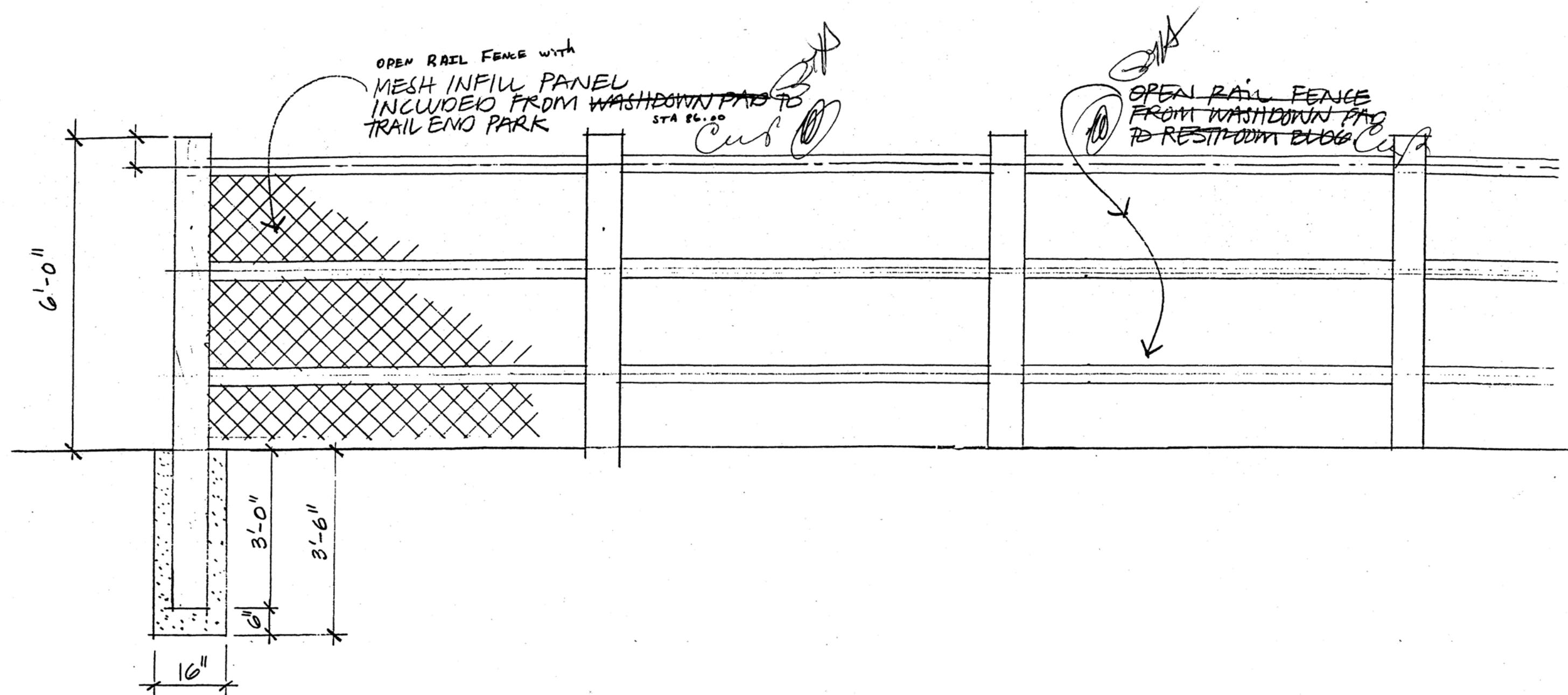
REVIEWS

NO. DATE DESCRIPTION

SHEET TITLE

DETAILS

SHEET NUMBER



LARRY SCOTT POST / RAIL FENCE DETAIL

$1/2" = 1'-0"$

5-28-97

NAKANO
DENNIS

CAUTION ENTERING INDUSTRIAL SHIPIARD

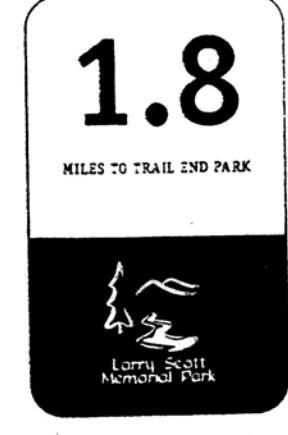
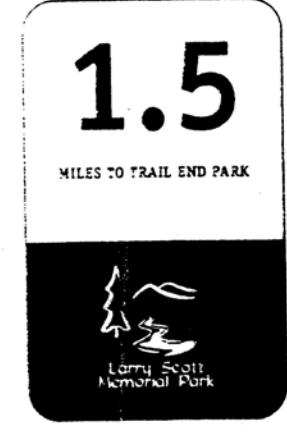
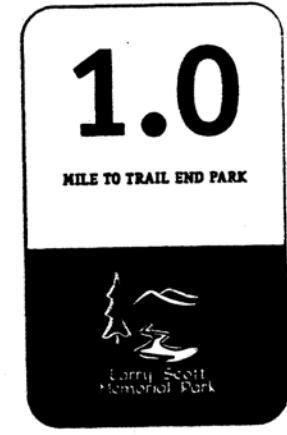
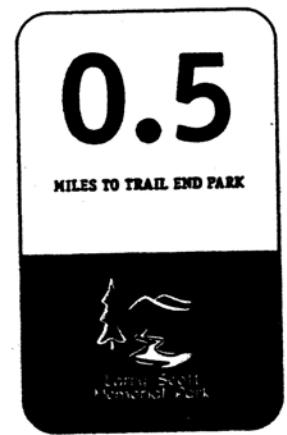
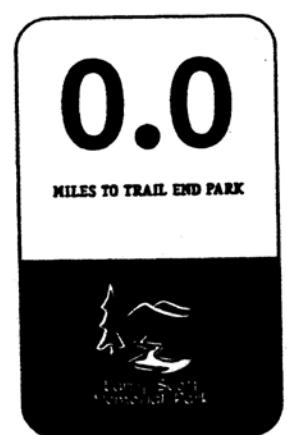
BICYCLISTS DISMOUNT AND
WALK TO WASHINGTON STREET.

Trail Caution Sign

(18" x 18", quantities noted, 1/2" rounded corners,
black foreground on bright orange background)

*NOTE: "Trail End Park" to be
named in accordance with
SECTION 4.3 of the Agreement.*

Ces



2

2

2

4

2

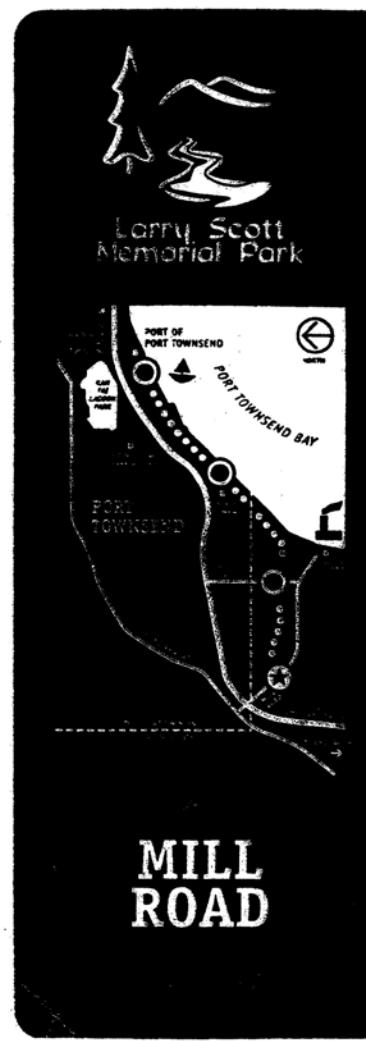
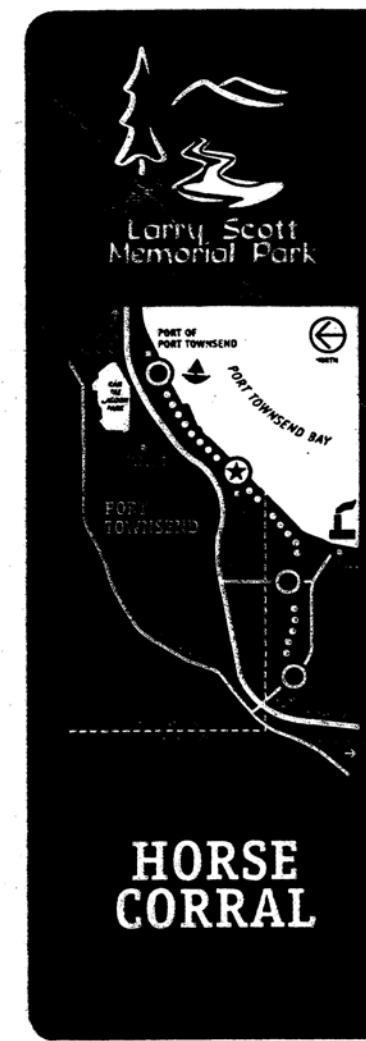
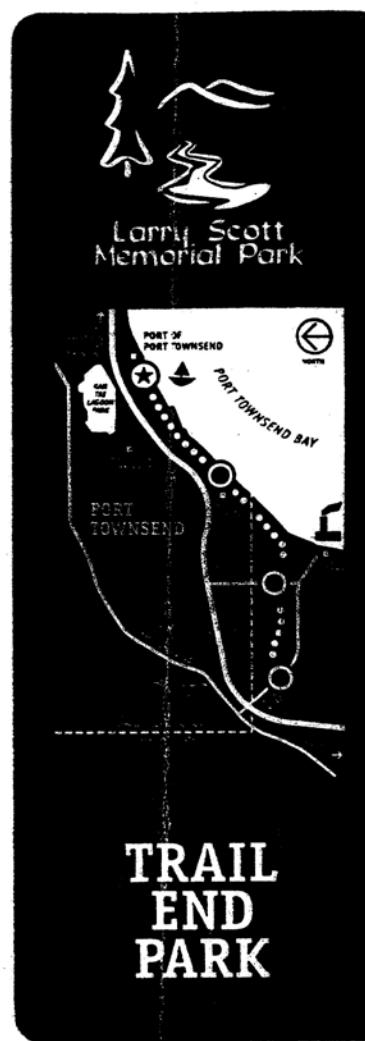
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1

1

2

1



Trail Locator Map Signage

(6 1/2" x 18", quantities noted, 1/2" rounded corners)

Trail Locator Mileage Markers

(4" x 6 1/2", 1/2" rounded corners, quantities as noted)

1

**END
OF
TRAIL**



1

**ENTERING
PORT OF
PORT
TOWNSEND**



1

**ENTERING
TRAIL END
PARK**

CYCLISTS DISMOUNT



1

**NO
HORSES
BEYOND
THIS
POINT**



2

CORRAL

4



4

5



5

**NO
MOTORIZED
VEHICLES
ALLOWED
ON TRAIL**



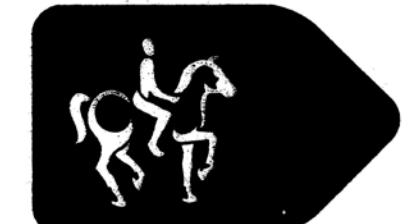
4



1

Trail User Signage (4" x 2 1/2", 5/16" rounded corners, quantities as noted)

4



4

4



4

**Trail Regulation Signage
(6 1/2" x 9", quantities noted,
1/2" rounded corners)**

Trail User Signage (6 1/2" x 4", 1/2" rounded corners, quantities as noted)

JEFFERSON COUNTY EXC^{T&A}

Aff. No. 082741

Date Paid 9/2/97 Amt. 0

By S. Hathaway, Deputy
TREASURER

After Recording Mail to:

POR TOWNSEND PAPER CORP.

750 ERICKSEN AVENUE NE

BAINBRIDGE ISLAND, WA 98110

Attn:

Please print or type information

403728

97-1-0033

Document Title(s) (or transaction contained therein):

1. QUIT CLAIM DEED
- 2.
- 3.
- 4.

Reference Number(s) of Documents assigned or released:

(on page of document(s))

Grantor(s) (Last name first, then first name and initials):

1. PORT TOWNSEND PAPER CORP.
- 2.
- 3.
- 4.
5. Additional names on page of document

CHARTER TITLE CORPORATION
has placed this document of record
as an accomodation only and
therefore accepts no liability for
the accuracy or validity of this
document.

Grantee(s) (Last name first, then first name and initials):

1. CITY OF PORT TOWNSEND
- 2.
- 3.
- 4.
5. Additional names on page of document

Legal Description (abbreviated: i.e. lot, block, plat or section, township, range):

SECTION 10, TOWNSHIP 30 NORTH, RANGE 1 WEST

Additional legal description is on page 2 of document

Assessor's Property Tax Parcel/Account Number:

001 104 017 001 104 011 & 001 104 000

The Auditor/Recorder will rely on the information provided on the form. The staff will not read the document to verify the accuracy or completeness of the indexing information provided herein.

rev 10/96 cover

RECORDED IN
VOL. PAGE
OF OFFICIAL RECORDS
REQUEST OF

97 SEP 12 PM 5:03

DONNA M. ELDRIDGE
JEFFERSON COUNTY AUDITOR

TY DEPUTY

Attachment C

Name Port Townsend Paper Corporation
Address 750 Erickson Avenue NE
Bainbridge Island, WA 98110
City, State, Zip _____

97-1-0033

Quit Claim Deed

Reference # (if applicable): _____
Grantor(s): (1) Port Townsend Paper Corp (2) _____
Grantee(s): (1) City of Port Townsend (2) _____
Additional Grantor(s) on pg. _____ Additional Grantee(s) on pg. _____
Legal Description (abbreviated): Section 10, Township 30 north, Range 1 West, W.M.
Parcels H.I.J.K.L.M.N.O.P.Q Additional legal(s) on page (See exhibit
Assessor's Tax Parcel ID# _____)

THE GRANTOR Port Townsend Paper Corporation, A Washington
Corporation.
for and in consideration of (Gift)

conveys and quit claims to The City of Port Townsend
the following described real estate, situated in the County of Jefferson
Washington, together with all after acquired title of the grantor (s) therein.

State of

(See attached Exhibit 'A')
CHARTER TITLE CORPORATION
has placed this document of record
as an accomodation only and
therefore accepts no liability for
the accuracy or validity of this
document.



Dated 19

(Individual)

(Individual)

Port Townsend Paper Corporation
By E. P. Swain _____
(President)
By Barbara L. Saitheasden _____
(Secretary)

STATE OF WASHINGTON
COUNTY OF Jefferson
On this day personally appeared before me
to me known to be the individual described in and who
executed the within and foregoing instrument, and
acknowledged that _____ signed the same as
free and voluntary act and deed, for the
uses and purposes therein mentioned.

GIVEN under my hand and official seal this _____
day of 19 _____

Notary Public in and for the State of Washington,
residing at _____
My appointment expires: _____

STATE OF WASHINGTON
COUNTY OF Jefferson
On this 25 day of March, 19 ⁹⁷
before me, the undersigned, a Notary Public in and for the State of
Washington, duly commissioned and sworn, personally appeared
E. P. Swain _____ Barbara L. Saitheasden _____
to me known to be the _____ President and _____
Secretary, respectively, of PORT TOWNSEND PAPER CORP.
the corporation that executed the foregoing instrument, and acknowledged
the said instrument to be the free and voluntary act and deed of said
corporation, for the uses and purposes therein mentioned, and on oath
stated that they are _____ authorized to execute the said instrument
and that the seal affixed is the corporate seal of said corporation.

Witness my hand and official seal hereto affixed the day and year
first above written.
Mary Norton _____ Mary Norton _____
Notary Public in and for the State of Washington
residing at Port Townsend, WA
My appointment expires: 2-14-98



Transnation

EXHIBIT A

Parcel H:

All of that portion of the Railroad right of way over and across that portion of Sections 10 and 15, Township 30 North, Range 1 West, W.M., described as follows:

Beginning at a point 40 feet East of the Southeast corner of Block 215 of Supplemental Plat of Eisenbeis Addition, as per plat recorded in Volume 2 of Plats, page 24, records of Jefferson County, Washington; thence West, along the North side of First Street, (now 6th Street) extended 838 feet to the Southeast corner of Block 241 of said Addition; thence South 532 feet to Southeast corner of Block 266 of said Addition; thence West 266 feet to Southeast corner of Block 265 of said Addition; thence South 266 feet to Southeast corner of Block 274 of said Addition; thence West 266 feet to Southeast corner of Block 275 of said Addition; thence South 266 feet to Southeast corner of Block 294 of said Addition; thence West 266 feet to Southeast corner of Block 293 of said Addition; thence South 838 feet, more or less, to the meander line on the Beach of Port Townsend Bay, and thence Northeasterly along said meander line to the place of beginning, and lying and being in Jefferson County, Washington.

EXCEPTING THEREFROM any portion thereof within Section 15, Township 30 North, Range 1 West, W.M.

Parcel I:

All that portion of the Railroad right of way over and across first class Tidelands District No. 112, as shown upon the official maps of Port Townsend Tidelands in Jefferson County, Washington on file in the Office of the Commissioners of Public Lands at Olympia, Washington. The width of said right of way is 50 feet.

Parcel J:

All that portion of the Port Townsend Railroad, Inc., right of way, over and across Hill Avenue water way, Washington Street, Water Street, Front Street and portion seaward of the Inner Harbor Line as shown upon the official maps of Port Townsend tidelands in Jefferson County, Washington, or file in the office of the Commissioner of Public Land at Olympia, Washington.

Parcel K:

An easement for railway purposes over all that portion of the Port Townsend Railroad right of way, over and across Tideland Tax A in Section 10, Township 30 North, Range 1 West, as granted by easement recorded in Volume 138 of Deeds, page 248, under Auditor's File No. 142657, records of Jefferson County, Washington.

EXHIBIT A continues...

Parcel L:

All of that portion of the Port Townsend Railroad, Inc., right of way, as conveyed by instrument recorded in Volume 91 of Deeds, page 98, under Auditor's File No. 52241, over and across that portion of Tideland District No. 111, being a strip of land 50 feet in width, having 25 feet of width on each side of the following described centerline;

Beginning at a point on the West line of said District, which is 318 feet South of the Northwest corner thereof, and running thence North 44° East, to a point on the East line of Section 10, Township 30 North, Range 1 West, W.M., which is North 0°26' East, 190.42 feet distant from the meander corner on said East line.

EXCEPTING THEREFROM, that portion of said premises conveyed by the State of Washington to the Milwaukee Terminal Railway Company by deed recorded in Volume 83 of Deeds, page 544, under Auditor's

File No. 39915.

ALSO EXCEPTING THEREFROM that portion of said premises conveyed to the Port of Port Townsend, by deed recorded under Auditor's

File No. 236640.

ALSO EXCEPTING THEREFROM that portion of said premises conveyed to Mark and Laura Burn by deed recorded under Auditor's File No.

305642.

Parcel M:

All of that portion of Chicago, Milwaukee, St. Paul and Pacific Railroad Company right of way conveyed by instrument recorded in Volume 91 of Deeds, page 22, under Auditor's File No. 51944, over and across that portion of Tideland District No. 111, being a strip of land 50 feet in width, having 25 feet of such width on each side of the following described centerline:

Beginning at a point on the produced East line of Section 10, Township 30 North, Range 1 West, W.M., which is South 0°26' West, 1016 feet distant from the meander corner on said East line and running thence South 67°46' West, 112.4 feet;

thence on a 12° curve to the right, turning through an angle of 37°22' a distance of 311.4 feet; thence North 74°52' West, 11.2 feet to the True Point of Beginning of this description, being a point on the center line of the tract conveyed by the State of Washington, to Milwaukee Terminal Railway Company by deed recorded in Volume 83 of Deeds, page 544, under Auditor's

File No. 39915;

thence on a 30°24' curve to the right, turning through an angle of 12°41' a distance of 42.4 feet;

thence North 62°11' West 63.3 feet;

thence on a 15°30' curve to the right, turning through an angle of .93°11' a distance of 601.8 feet;

thence North 31° East 38.6 feet;

thence on a 30°24' curve to the right, turning through an angle of 12°41', a distance of 42.4 feet to a point on the center line of the tract conveyed by the State of Washington to Puget Sound Navigation Company by deed recorded in Volume 91 of

Deeds, page 97, under Auditor's File No. 52241.

EXCEPTING THEREFROM that portion of said premises included within the conveyances to the Milwaukee Terminal Railway Company and to Puget Sound Navigation Company as herein set forth.

EXHIBIT A continues...

EXHIBIT A (page 3)

Parcel N:

All of that portion of the Chicago, Milwaukee, St. Paul and Pacific Railroad Company right of way over and across that portion of the Triangular tract of Tideland District No. 111, lying between and bounded by a tract of land conveyed to Milwaukee Terminal Railway Company, recorded in Volume 83 of Deeds, page 544, under Auditor's File No. 39915; by a tract of land conveyed to Puget Sound Navigation Company, recorded in Volume 91 of Deeds, page 97, under Auditor's File No. 52241; and by a tract of land conveyed to the Port Townsend and Puget Sound Railway Company, recorded in Volume 91 of Deeds, page 22, under Auditor's File No. 51944; said railroad right of way was conveyed by instrument recorded in Volume 91 of Deeds, page 21, under Auditor's File No. 51943.

Parcel O:

All of that portion of the Chicago, Milwaukee, St. Paul and Pacific Railroad Company right of way, over and across that portion of Tideland District No. 111, being 100 feet in width, having 50 feet of such width on each side of the following described centerline.

Beginning at a point on the East line of Hill Avenue Waterway 213 feet South of the Northeast corner of said waterway, and running thence North 44° East 8 feet; thence on a 12° curve to the right, turning through an angle of 61°08', a distance of 509.4 feet; thence South 74°52' East, 383.8 feet; thence on a 12° curve to the left, turning through an angle of 37°22', a distance of 311.4 feet; thence North 67°46' East, 112.4 feet to a point on the Section line between Sections 10 and 11, Township 30 North, Range 1 West, W.M., produced, said point being South 0°26' West, 1016 feet from the meander corner to said Sections 10 and 11. EXCEPTING THEREFROM any portion of Front and Water streets which may be described in the tidelands above.

EXHIBIT A continues...

Parcel P:

That portion of Section 10, Township 30 North, Range 1 West, described as follows:

Beginning at the Northwest corner monument of Tax Lot No. 3, of said Section 10, said monument being 339.3 feet, South $0^{\circ}14'20''$ East, of the centerline of State Highway No. 9; thence South $0^{\circ}34'20''$ East a distance of 217.77 feet; thence South $64^{\circ}52'10''$ East, a distance of 153.4 feet; thence North $47^{\circ}55'$ East, a distance of 100 feet; thence North $48^{\circ}51'$ East, a distance of 85.00 feet; thence South $41^{\circ}09'$ East, a distance of 10.0 feet to the True Point of Beginning;

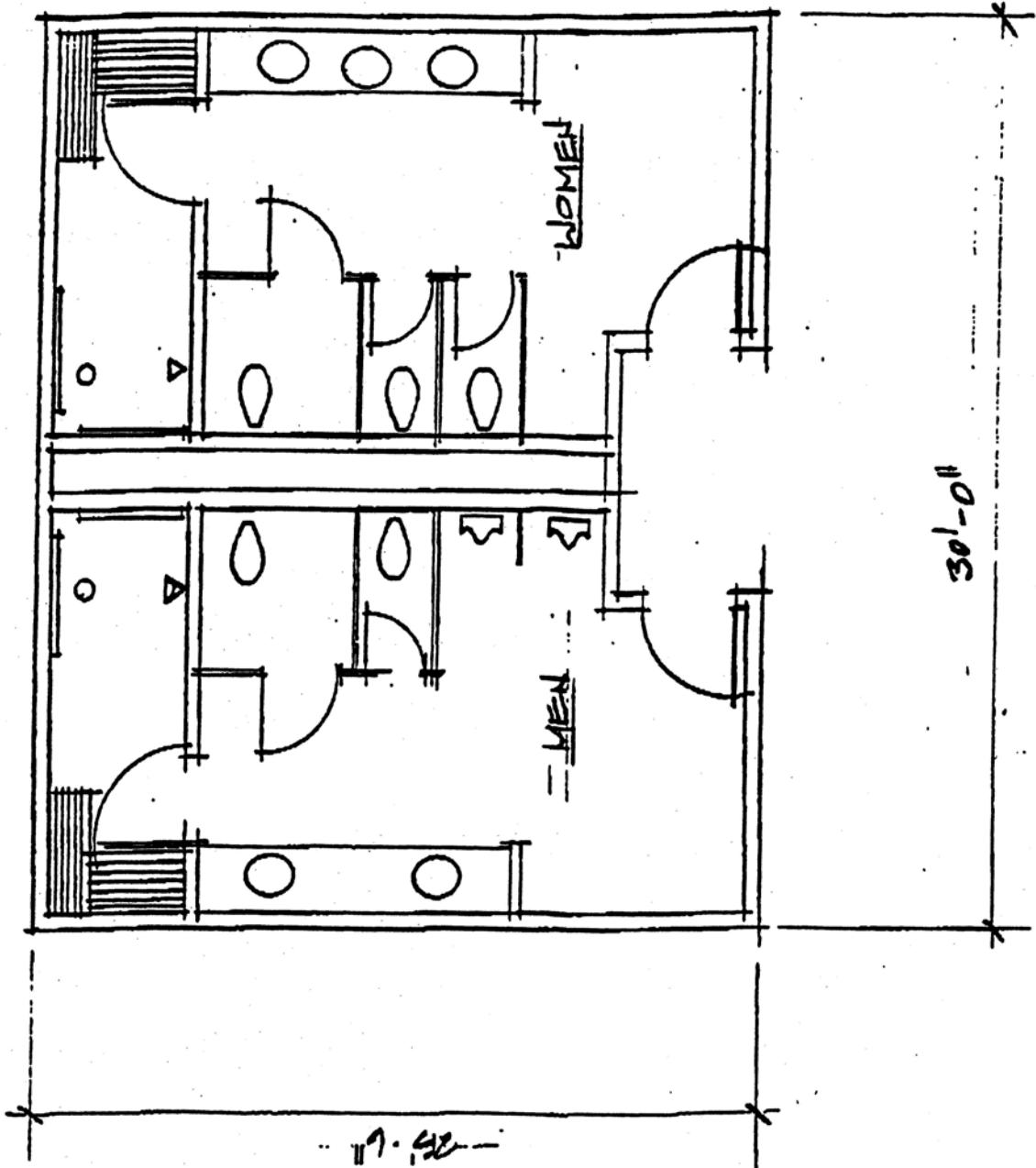
thence North $48^{\circ}51'$ East, a distance of 15.0 feet; thence North $51^{\circ}33'$ East, a distance of 100.0 feet; thence North $52^{\circ}21'$ East, a distance of 100.0 feet; thence North $53^{\circ}33'$ East, a distance of 100.0 feet; thence North $53^{\circ}51'$ East, a distance of 140.7 feet thence North $0^{\circ}33'10''$ East, along said section line a distance of 49.9 feet; thence South $53^{\circ}51'$ West, a distance of 170.6 feet; thence South $53^{\circ}33'$ West, a distance of 21.0 feet, more or less to the Southerly line of existing property of Chicago, Milwaukee, St. Paul and Pacific Railroad Company; thence South $44^{\circ}14'$ West, a distance of 297.0 feet more or less to the True Point of Beginning, all lying in Tax Lot No. 3, Section 10, Township 30 North, Range 1 West, W.M., in Jefferson County, Washington.

Parcel Q:

All that portion of first class Tidelands District No. 112, lying Northerly of the railroad right of way over and across said tidelands, as shown upon the official maps of Port Townsend Tidelands in Jefferson County, Washington, on file in the Office of the Commissioner of Public Lands at Olympia, Washington, and as described in this Exhibit A as Parcel I.

END EXHIBIT A

NOTE: Parcels H through P, inclusive, are so designated to conform to the designations set forth in that certain instrument recorded in Volume 246, pages 515 through 521, inclusive, under Auditor's No. 310886, records of Jefferson County, Washington.



TEST ROOM

30'-0"

11'-4"

Attachment D

City of Port Townsend
Building and Community Development
540 Water Street, Port Townsend, WA 98368
360/385-3000 FAX 360/385-4290



**MITIGATED DETERMINATION OF NONSIGNIFICANCE
AND LEAD AGENCY STATUS**

Description of Proposal: The proposed project includes the acquisition and future construction of a recreational trail for hikers, equestrians and non-motorized vehicles. Much of the trail is to be constructed along an abandoned railroad right-of-way. The completed trail will be approximately 7 miles in length. However, approximately only 0.75 miles of the total trail will be located within the city limits of Port Townsend, with the remainder of the trail located within Jefferson County.

Proponent: Jefferson County Public Works Department
Jefferson County Courthouse, P.O. Box 1220, Port Townsend, WA 98368
Authorized representative: Mium Rubin-Crump

Location of Proposal: The trail is proposed to begin at the Jefferson Transit Park and Ride facility located at the intersection of 12th and Landis Streets, adjacent to and northerly of the Safeway store, crossing SR 20 and through the Port of Port Townsend properties to the Port Townsend Paper Mill in Jefferson County, Section 10, Township 30 North, Range 01 West.

Lead Agency: City of Port Townsend

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An environmental impact statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information in file with the lead agency. This information is available to the public on request.

This MDNS is issued under 197-11-340(2); the lead agency will not act on this proposal for 15 days from the date below. Comments must be submitted by June 27, 1996.

Responsible Official: David Robison, Director of Building and Community Development
City Hall, 540 Water Street
Port Townsend, Washington 98368
(206) 385-3000

Date: June 12, 1996

Signature:



David Robison, Director
Building and Community Development

CITY OF PORT TOWNSEND
Environmental Determination
Building & Community Development Director

DATE: June 12, 1996

PROPOSAL: The proposed project includes the acquisition and future construction of a recreational trail for hikers, equestrians and non-motorized vehicles. Much of the trail is to be constructed along an abandoned railroad right-of-way. The completed trail will be approximately 7 miles in length. However, approximately only 0.75 miles of the total trail will be located within the city limits of Port Townsend, with the remainder of the trail located within Jefferson County.

LOCATION: The trail is proposed to begin at the Jefferson Transit Park and Ride facility located at the intersection of 12th and Landis Streets, adjacent to and northerly of the Safeway store, crossing SR 20 and through the Port of Port Townsend properties to the Port Townsend Paper Mill in Jefferson County, Section 10, Township 30 North, Range 01West.

APPLICANT: Jefferson County Public Works Department
Authorized representative: Mium Rubin-Crump

FILE REFERENCE: SEPA file 9605-04(S)

RESPONSIBLE OFFICIAL: Dave Robison, Director
Building & Community Development

THRESHOLD DETERMINATION: This is a Mitigated Determination of Non-Significance

TO: All Permit and Review Authorities

ENVIRONMENTAL RECORD

The environmental review consisted of analysis based on the following documents included in the environmental record.

- Environmental Checklist dated May 8, 1996 (*see Exhibit 1, Environmental Checklist*).
- Larry Scott Memorial Trail Priority Habitat and Species Report, prepared by Knuz Biological Consulting, (no date).
- Wetlands Report, prepared by Pacific Rim Soil and Water, dated July 3, 1995.

References:

- * Urban Waterfront Plan, December 1990
- * Draft EIS (Volume II) for the Urban Waterfront Plan, September 1990
- * US SCS Jefferson County Soil Survey, 1978
- * 1981 Port Townsend Comprehensive Plan
- * 1992 Department of Ecology Stormwater Management Manual for the Puget Sound Basin
- * City of Port Townsend Environmentally Sensitive Areas Maps, August 5, 1993
- * Title 19, Environmental Protection, Port Townsend Municipal Code

Unless otherwise noted, the above information is available for review at the Building & Community Development Department, Port Townsend City Hall, 540 Water Street, between the hours of 9:00 AM to 4:00 PM Monday through Friday.

RESPONSIBLE OFFICIAL'S AMENDMENTS TO THE ENVIRONMENTAL CHECKLIST

The following sections correspond with related categories of the environmental checklist submitted for the proposal and clarify, amend, or add to that document.

I. PROPOSAL DESCRIPTION:

The proposed project includes the acquisition and future construction of a recreational trail for hikers, equestrians and non-motorized vehicles. Much of the trail is to be constructed along an abandoned railroad right-of-way. The completed trail will be approximately 7 miles in length. However, only approximately 0.75 miles of the total trail will be located within the city limits of Port Townsend, with the remainder of the trail located within Jefferson County (*see Exhibit 2, Project Location map*).

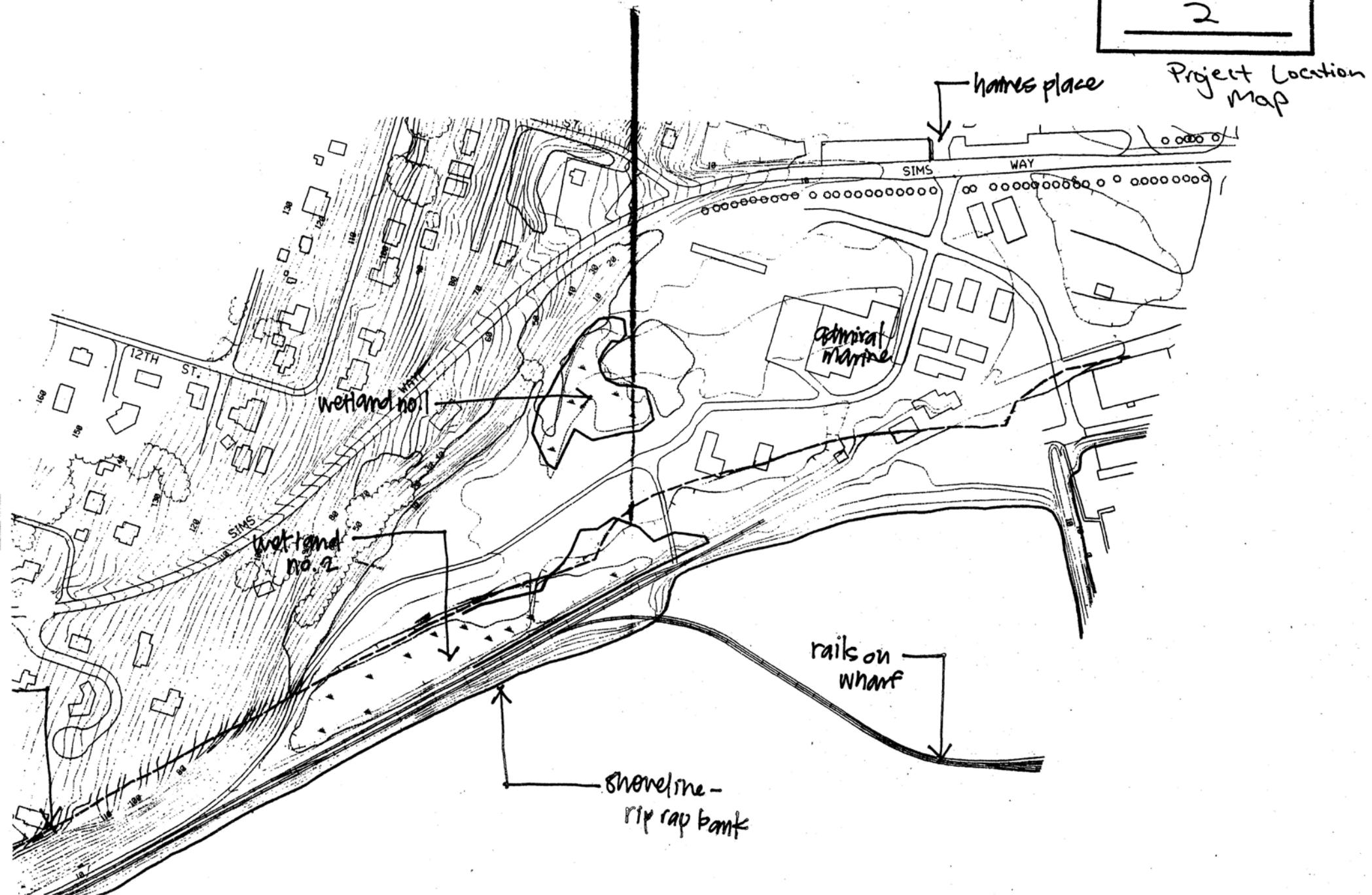
The trail is proposed to begin at the Jefferson Transit Park and Ride facility located at the intersection of 12th and Landis Streets, adjacent to and northerly of the Safeway store, crossing SR 20 and through the Port of Port Townsend properties to the Port Townsend Paper Mill in Jefferson County. This environmental determination evaluates three alternative locations for placement of the trail through the Port's industrial area to the paper mill (*see Exhibit 3, Alternatives for Preliminary Shoreline Trail Plan*).

The average maximum width of the trail right-of-way will be thirty feet, including all proposed vegetative buffers. However, those areas located within environmentally sensitive areas may require increased vegetative buffers. An optimal layout for the proposed pedestrian/bicycle trail is a 12 foot wide primary path with two foot wide shoulders on either side (*see Exhibit 4, Trail Sections 1,2, &3*).

EXHIBIT

2

Project Location
Map



II. PERMITS REQUIRED:

- * Hydraulic Permit Approval, Washington State Department of Fish and Wildlife
- * Section 404 Nationwide Permit, U.S. Army Corp of Engineers
- * Environmentally Sensitive Areas Permit, City of Port Townsend
- * Shoreline Substantial Development Permit, City of Port Townsend
- * Clearing and Excavation Permit, City of Port Townsend

III. CHECKLIST ITEMS

A. EARTH

According to the Soil Survey of the Jefferson County Area, Washington (USDA, 1973), the soil located on the site is low-lying, cut and fill land. It consists of a mixture of many types of soil material.

Minimal grading will be required for path construction. A balanced cut and fill is proposed. Potential impacts of soil erosion can be limited by a variety of erosion and sedimentation control measures during construction. The proposal should incorporate best management practices from the Department of Ecology Puget Sound Water Quality Manual to minimize erosion during construction.

Alternative C would have the least environmental impact as all earth disturbances would occur primarily along the existing railroad grade or within developed areas of the Port's boatyard and would require less grading and fill than the other two alternatives. Alternative A would have more of a moderate impact because a portion of the trail construction would likely occur in the edges of the buffers of wetland #1 and #2. In addition Alternative A would require more grading and fill to raise the trail above the existing grade. Alternative B would have the highest impacts due to earth disturbances at the base of the bluff and at the southern and eastern edges of wetland #1.

B. AIR

The environmental checklist description is adequate.

Odors from the Port Townsend Paper Mill would be similar to all three of the alternatives, however, Alternative C may have less impacts from odors because it is directly abutting the shoreline where there is better air circulation and breezes off Port Townsend Bay.

C. WATER

Potential impacts of soil erosion and water quality can be limited by a variety of erosion and stormwater control measures during construction. The proposal should incorporate best management practices from the Department of Ecology Puget Sound Water Quality Manual to minimize erosion during construction and provide adequate stormwater control.

Alternative C would have the least impact to stormwater quantities and potential adverse water quality impacts due to the use of existing, elevated railroad grade for the majority of the trail and its northwest connection to Boat Street which is linked to the Port of Port Townsend's centralized stormwater system. Both Alternative A and B would have much higher impacts due to the construction of new imperious surfaces, thereby creating the need for additional stormwater facilities. In addition, the close proximity of Alternative A and B to both wetland #1 and #2 may result in a higher potential for water quality impacts to the wetlands due to stormwater discharges. In addition, Alternative A would likely have slightly less impact than Alternative B as a portion of the trail route would be along the existing dirt road while a portion of Alternative B would involve new construction along the base of a very steep bluff and be located within the southern buffer of wetland #1.

D and E. PLANTS AND ANIMALS

A wildlife and critical habitat survey was conducted to assess the area for critical wildlife habitat and to determine if priority species are present (see Kunz Biological Report). The report identifies the shoreline area as having fairly extensive eel grass beds which are utilized by black brant and harlequin ducks for feeding and generally supports high wildlife diversity and density. In addition, a pigeon guillemot nesting area occurs toward the eastern edge of the project area in the cut bank above the railroad bed. The report concludes the proposed project is unlikely to have a significant impact to priority habitats or species provided the cut bank remains intact for pigeon guillemot nesting.

A wetland delineation and report was conducted for wetland #1 and #2 (see Pacific Rim and Soil Report). The report states that wetland #1 is a small, less than 1 acre, wetland with poor water quality, low species diversity, minimal buffers, and with signs of ongoing impacts from the adjacent port industrial area. Wetland #2 is approximately 2-3 acre open water wetland with relatively high species diversity and minimal buffers due to the abutting railroad bed and existing dirt road to the north. Both wetlands have been rated as Category III wetlands and require a fifty foot buffer under the City's environmentally sensitive area standards.

A Sensitive Area Permit is required by Title 19, Environmental Protection, if any portion of the development is within a sensitive area or its buffer. The site has both wetlands and steep slopes, both defined as sensitive areas under Title 19. Buffers may be reduced on a case by case basis, but in general, only if there is no detrimental effect on the wetland or the stability of a steep slope. Under Title 19, new development on sites within wetland areas shall include specific design and maintenance measures to avoid general impacts (e.g., littering and vegetation removal) by protecting or enhancing buffer areas, discouraging access, minimizing exotic plantings outside the wetland area, etc. In steep slope areas, a minimum buffer of 25 feet shall be provided from the edge of all slopes and marine bluffs that are subject to landslide or erosion hazards.

Alternative C would likely have the least potential impact to plants and animals because the trail would be constructed on an existing railroad bed. However, due to the existing railroad bed being at a higher elevation to wetland #2, there could be disruptions to shorebird's use of the open water area of the wetland for breeding, feeding and resting purposes, by passing users of the trail. Both Alternative A and B would require substantially more clearing and grading, and some fill, to prepare the grade for construction of the trail and would present higher vulnerability to habitat alteration and species displacement. Alternative B also poses more probable impacts to the habitat contained in the bluff area, which provides a travel corridor and nesting or resting place for shorebirds. Alternative B would also require further geotechnical study to prevent slope instability by construction of the trail at the toe of the bluff. Title 19 requires a 25 foot setback from the toe of the bluff.

F. ENERGY AND NATURAL RESOURCES

The environmental checklist description is adequate.

G. ENVIRONMENTAL HEALTH

The environmental checklist description is adequate.

H. NOISE

Construction should be limited between 8:00 am and 6 pm, Monday through Friday, unless prior approval is granted by the Director for special circumstances.

I. LAND AND SHORELINE USE

The proposal site is located within the Port Townsend Urban Waterfront Special District of the Port Townsend Shoreline Master Program (SMP). The site is within an urban designation under the SMP. The SMP contains specific criteria for public access improvements. In addition, the Urban Waterfront Plan recommends that a habitat and environmental quality monitoring station could be established in the wetlands area of the proposal site; that all vehicular traffic through the urban wetland district should be prohibited, and; the city should develop a wetlands protection ordinance that establishes wetland buffer zones (see above discussion regarding wetlands standards under Section D & E, Plants and Animals; the site is shown as an environmentally sensitive area on the City's ESA maps due to potential flooding, steep slopes and wetland areas).

No parking standards are set forth in Chapter 17.30 PTMC for publicly-oriented trails. Off-street parking requirements should be considered during the shoreline permit process.

J. HOUSING

The proposed application will not displace or provide any housing.

K. AESTHETICS

The environmental checklist description is adequate.

L. LIGHT AND GLARE

The environmental checklist description is adequate.

M. RECREATION

The environmental checklist description is adequate.

N. HISTORICAL AND CULTURAL PRESERVATION

The City is not aware of any cultural or historical resources existing on this site. In the event that suspected cultural materials are found during clearing or excavation, the Building & Community Development Director and the Washington State Office of Archaeology and Historical Preservation should be notified immediately, and construction temporarily stopped.

O. TRANSPORTATION

The description in the checklist of how the site is served by the larger transportation network is adequate. However, the proposal should include bicycle and pedestrian improvements to the intersection of SR 20 and Haines Street to facilitate safe travel for the users of the trail.

No parking standards are set forth in Chapter 17.30 PTMC for publicly-oriented trails, therefore no parking is proposed by the applicant. Trail user parking could be accommodated at the Jefferson Transit Park and Ride facility. Nonetheless, off-street parking requirements will be considered during the shoreline substantial development process.

Alternative A and B are poorly connected to the existing sidewalks and crosswalks at the intersection of Haines and SR 20. Alternative C provides for better pedestrian circulation and bicycle crossing at this intersection. However, Alternative C would require careful design and planning in the and through the Port boatyard facility to manage potential safety impacts between trail users and boatyard uses. There is inadequate information to determine if there would be of safety impacts between port use and trail users. Alternative C would also create more demand and potential vehicle safety conflicts along Boat Street due to the increased number of pedestrians and bicyclists using the trail along this route.

P. PUBLIC SERVICES

Jefferson County Department of Public Works will undertake any maintenance activities associated with the upkeep of the trail. Alternative C would likely have a greater impact on the need for public services by the Port to monitor users of the trail in those areas where conflicts between Port boatyard uses and trail use may conflict.

Q. UTILITIES

A restroom building is proposed at the southwest corner of Haines Place. No other utilities will be provided to the trail and project area.

SUMMARY OF REVIEW OF THE THREE ALTERNATIVES (A, B & C)

The purpose for any of the proposed alternatives is to provide a trail connection from the existing railroad right-of-way to and through the Port of Port Townsend's industrial area to the intersection at SR 20 and Haines Street and to connect to the Jefferson Transit park and ride lot just to the north of the intersection.

Based on the above discussion and the summary chart (below) comparing the relative potential impacts associated with each of the proposed alternatives, Alternative C would have the least probable significant adverse impacts to the natural environment (earth, water, plants and animals) because the trail would be constructed on an existing railroad bed and would not involve any significant clearing and grading of undisturbed natural areas, or within wetland areas or their buffers, or close to the toe of the slope.

It could be argued that with careful design and site specific mitigation most of the probable significant adverse environmental impacts associated with Alternative A and B may be avoided. However, Alternative A would require further detailed design and a geotechnical report to address potential slope stability problems associated with construction of the trail at the toe of the bluff. In addition, an environmentally sensitive areas permit would be required to assess the exact location, placement and probable significant adverse environmental impacts of trail construction within the buffer of wetland #1 for both Alternative A and B.

Under both Alternative A and B, trail construction should avoid, if feasible, any construction activities or placement of impervious surfaces within the wetland or its buffers. Due to the Port's proposed boatyard expansion, it appears that there may not be sufficient space to locate and construct the portion of the trail along the eastern edge of wetland #1 as proposed by Alternative A and B without more detailed study of the trail alignment and the exact location of the edge of the wetland buffer. Further study may reveal that the trail could only be located along this route if the Port's proposed expansion of the boatyard in this area was reduced to allow sufficient room for the placement of the trail.

SUMMARY COMPARISON OF PROBABLE SIGNIFICANT ADVERSE ENVIRONMENTAL IMPACTS OF THE THREE ALTERNATIVES FOR THE LARRY SCOTT MEMORIAL TRAIL PROPOSAL

	Alternative A	Alternative B	Alternative C
Earth	Moderate	Highest	Least
Air	Moderate	Moderate	Least
Water:			
Stormwater	Moderate	Highest	Least
Ground Water	N/A	N/A	N/A
Plants:	Moderate	Highest	Least
Animals	Moderate	Highest	Least
Energy & Natural Resources	N/A	N/A	N/A
Environmental Health	N/A	N/A	N/A
Noise	Minimal	Minimal	Minimal
Land and Shoreline Use	Least	Medium	Highest
Housing	N/A	N/A	N/A
Aesthetics	N/A	N/A	N/A
Light and Glare	No impacts	No impacts	No impacts
Recreation	No impacts	No impacts	No impacts
Historical and Cultural Preservation	No known impacts	No known impacts	No known impacts
Transportation	Least	Moderate	Highest
Public Services	Least	Moderate	Highest
Utilities	N/A	N/A	N/A

CONCLUSIONS AND ENVIRONMENTAL DETERMINATION

The environmental review indicates a probability of significant adverse environmental impacts from the proposal, which could be mitigated by construction of Alternative C.

The following recommendations and conditions are intended to mitigate probable significant adverse environmental impacts disclosed during SEPA review. Based on information presented in the environmental checklist and this environmental determination report, issuance of a Mitigated Determination of Non-Significance is appropriate.

This threshold determination is subject to the following mitigation measures.

MITIGATION MEASURES:

General:

1. Alternative C shall be the preferred action for construction of the Larry Scott Memorial Trail proposal. In the event that Jefferson County does not proceed with Alternative C, additional detailed environmental study and a sensitive area permit shall be required for either Alternative A or B.

A. EARTH

2. To mitigate possible soil and water erosion hazards, the applicant shall incorporate temporary erosion and sediment control measures and best management practices contained in the Department of Ecology Stormwater Management Manual (DOE SWMM) to control the rate of runoff and erosion from the construction site.
3. Project activities shall be conducted according to best management practices contained in the DOE SWMM to minimize siltation of shoreline and wetland areas.

WATER

4. If any rip rap repair work is required along the existing railroad bed, the applicant shall receive a Hydraulic Permit Approval from the Washington State Department of Wildlife and Fisheries. In addition, to mitigate possible water quality and turbidity impacts, the applicant shall obtain permit approval from the Washington Department of Ecology for Temporary Modification of Water Quality Criteria and any approval necessary from the U.S. Army Corp of Engineers.

D&E. PLANTS AND ANIMALS

5. A Sensitive Area Permit is required if any portion of the trail development is within a wetland or its buffer or is within the buffer of the steep slope.
6. Final trail design shall be setback as far as feasible from the base of the steep slope in the area of any pigeon guillemot nests.
7. The applicant shall prepare specific design and maintenance measures to be reviewed and approved by the Director of BCD prior to construction. The design and maintenance measures shall include provisions on how to avoid general impacts (e.g., littering and vegetation removal) by protecting or enhancing buffer areas, and discouraging access into wetland areas or on the slope of the bluff. In addition, the location and design of the wetland interpretive area and all signs shall also be reviewed and approved by the Director.

H. NOISE

8. To mitigate noise impacts on adjacent properties, construction activities and pile driving shall occur only between the hours of 8:00 a.m. to 6p.m., Monday through Friday, unless advance approval is given by the Director of Building and Community Development for unique circumstances.

I. LAND AND SHORELINE USE

9. Depending on the alternative chosen, additional specific conditions of development may be determined by appropriate conditions attached to the shoreline use permit or sensitive area permit. Permit approvals from other state or federal agencies shall be submitted to the city to be included in the project file.

N. HISTORICAL AND CULTURAL PRESERVATION

10. In the event that suspected cultural or archeological materials are found during clearing or excavation, the Building and Community Development Director and the Washington State Office of Archaeology and Historical Preservation shall be notified immediately, and construction temporarily stopped.

O. TRANSPORTATION

11. Depending on the Alternative chosen, the project should include bicycle and pedestrian improvements to the intersection of SR 20 and Haines Street to facilitate safe travel for the users of the trail. The proponents shall consult with Jefferson Transit, the Washington State Department of Transportation and the Public Works Director regarding the type of improvements to be required.

12. If Alternative C is chosen, the applicant shall coordinate the trail design, placement and safety measures through the Port's industrial area to minimize potential safety conflicts between port uses and trail users.

P. PUBLIC SERVICES

13. Jefferson County Department of Public Works shall be responsible for any maintenance or repair activities associated with the upkeep of the trail.

R. MISCELLANEOUS

14. A copy of these mitigation measures shall be given to and read by the project contractor and all sub-contractors prior to beginning construction, and a list of these mitigation measures shall be prominently posted at the site in a waterproof container or bag.

In consideration of the above, the Larry Scott Memorial Trail proposal is found to have a SEPA threshold determination of non-significance. This environmental determination does not excuse the applicant from complying with other local, state, and federal ordinances, regulations, or statutes applicable to the proposed development.



Dave Robison, SEPA Responsible Official,
Director, Building & Community Development
City of Port Townsend



HYDRAULIC PROJECT APPROVAL

RCW 75.20.100 or RCW 75.20.108

State of Washington
Department of Fish and Wildlife
Region 7 Office
600 Capitol Way North, MS 3155
Olympia, Washington 98501-1091

DATE OF ISSUE: August 17, 1998

LOG NUMBER: 00-D6521-01

<u>PERMITTEE</u>	<u>AUTHORIZED AGENT OR CONTRACTOR</u>
Jefferson County Department of Public Works ATTENTION: Dick Burnham P.O. Box 2070 Port Townsend, Washington 98368 (360) 385-9160 Fax: (360) 385-9234	Not Applicable

PROJECT DESCRIPTION: Construct Shoreline Recreational Trail; Shoreline Restoration (Mitigation)

PROJECT LOCATION: Port of Port Townsend Boat Haven, Port Townsend

#	<u>WRIA</u>	<u>WATER BODY</u>	<u>TRIBUTARY TO</u>	<u>1/4 SEC.</u>	<u>SEC.</u>	<u>TOWNSHIP</u>	<u>RANGE</u>	<u>COUNTY</u>
1	17.MARI	Port Townsend Bay	Admiralty Inlet	SE	10	30 North	01 West	Jefferson

NOTE: This Hydraulic Project Approval (HPA) does not constitute an after-the-fact HPA for work previously conducted below the ordinary high water line without a valid HPA, nor does it relieve the permittee from any legal responsibilities for that previously conducted work.

The work permitted under this HPA will involve placement of fill waterward of nonpermitted fill already placed waterward of the ordinary high water line by the Port of Port Townsend. The alignment of the trail shall require mitigation for which a specific site and full scope of work has yet to be determined.

PROVISIONS

- TIMING LIMITATIONS:** The project may begin **Immediately** and shall be completed by **March 1, 2000**, provided:
 - Work below the ordinary high water line shall not occur from **March 15** through **June 14** of any year for the protection of migrating juvenile salmonids.
- NOTIFICATION REQUIREMENT:** The permittee or contractor shall notify the Area Habitat Biologist (AHB) listed below of the project start date. Notification shall be received by the AHB at least three working days prior to the start of construction activities and upon completion of trail and mitigation work. The notification shall include the permittee's name, project location, starting date for work, and the control number for this Hydraulic Project Approval.
- The additional fill for the trail shall require full mitigation in the form of fill removal and shoreline restoration of an area not less than 700 square feet. The finalized location and restoration plan shall be reviewed and approved by WDFW prior to the start of construction, shall be attached to this permit, and work shall be completed within 18 months of the date of this permit.
- No portion of the trail shall extend waterward further than the approved alignment.



HYDRAULIC PROJECT APPROVAL

RCW 75.20.100 or RCW 75.20.108

State of Washington
Department of Fish and Wildlife
Region 7 Office
600 Capitol Way North, MS 3155
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DATE OF ISSUE: August 17, 1998

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5. Native and imported logs may be used to stabilize the waterward edge of the trail. Logs may be anchored into the beach. Native logs to be used must remain in their original position, or shall be replaced by equivalent-sized logs.
6. The side slopes of the trail shall be no steeper than 1.5 feet horizontal to one foot vertical and shall be revegetated with native vegetation. Vegetative replantings shall be maintained and monitored to achieve a minimum of 80 percent survival.
7. The mitigation /restoration area shall require removal of at least 700 cubic feet of material, providing beach nourishment with an approved material, revegetation, and recontouring the bankline to provide improved nearshore habitat.
8. Wet concrete shall be prevented from entering waters of the state. Forms for any concrete structure shall be constructed to prevent leaching of wet concrete. Impervious materials shall be placed over any exposed concrete not lined with the forms that will come in contact with state waters. Forms and impervious materials shall remain in place until the concrete is cured.
9. Use of wood products treated with preservatives shall not be allowed below the ordinary high water line.
10. All manmade debris on the beach shall be removed and disposed of upland such that it does not enter waters of the state.
11. Project activities shall not occur when the project is inundated by tidal waters.
12. Use of equipment on the beach shall not be allowed. All equipment shall operate from the uplands.
13. All construction materials shall be stockpiled on the uplands. No stockpiling of construction materials shall occur on the beach.
14. All excess excavated or construction material shall be removed from the beach within 72 hours of trail construction. Upon removal of the material, the beach shall immediately be returned to the preproject natural grade.
15. Beach area depressions created during project activities shall be reshaped to preproject beach level upon project completion.
16. Removal or destruction of bankline vegetation shall be limited to that necessary for the construction of the project.
17. Intertidal wetland vascular plants shall not be adversely impacted due to project activities. If such vegetation is adversely impacted, it shall be replaced using proven methodology.
18. All natural habitat features on the beach larger than 12 inches in diameter, including trees, stumps, logs, and large rocks, shall be retained on the beach following construction. These habitat features may be moved during construction if necessary.
19. Project activities shall be conducted to minimize siltation of the beach area and bed.
20. If a fish kill occurs or fish are observed in distress, the project activity shall immediately cease and WDFW Habitat Program shall be notified immediately.



HYDRAULIC PROJECT APPROVAL

RCW 75.20.100 or RCW 75.20.108

State of Washington
Department of Fish and Wildlife
Region 7 Office
600 Capitol Way North, MS 3155
Olympia, Washington 98501-1091

DATE OF ISSUE: August 17, 1998

LOG NUMBER: 00-D6521-01

21. All debris or deleterious material resulting from construction shall be removed from the beach area and bed and prevented from entering waters of the state.
22. No petroleum products or other deleterious materials shall enter surface waters.
23. Wood treated with preservatives, trash, waste, or other deleterious materials shall not be burned below the ordinary high water line. Limited burning of untreated wood or similar material may be allowed at or above the mean higher high water line.
24. Project activities shall not degrade water quality to the detriment of fish life.

SEPA: Exempt, no other non-exempt permits

APPLICATION ACCEPTED: August 6, 1998

ENFORCEMENT OFFICER: Henry 126 [P1]

James S. Brennan (360) 895-4757
Area Habitat Biologist

A handwritten signature of James S. Brennan.

for Director
WDFW

cc: Steve Keller, WDFW
Judy Serber, City of Port Townsend

GENERAL PROVISIONS

This Hydraulic Project Approval (HPA) pertains only to the provisions of the Fisheries Code (RCW 75.20). Additional authorization from other public agencies may be necessary for this project.

This HPA shall be available on the job site at all times and all its provisions followed by the permittee and operator(s) performing the work.

This HPA does not authorize trespass.

The person(s) to whom this HPA is issued may be held liable for any loss or damage to fish life or fish habitat which results from failure to comply with the provisions of this HPA.

Failure to comply with the provisions of this Hydraulic Project Approval could result in a civil penalty of up to one hundred dollars per day or a gross misdemeanor charge, possibly punishable by fine and/or imprisonment.

All HPAs issued pursuant to RCW 75.20.100 or 75.20.160 are subject to additional restrictions, conditions or revocation if the Department of Fish and Wildlife determines that new biological or physical information indicates the need for such action. The permittee has the right pursuant to Chapter 34.04 RCW to appeal such decisions. All HPAs issued pursuant to RCW 75.20.103 may be modified by the Department of Fish and Wildlife due to changed conditions after consultation with the permittee: PROVIDED HOWEVER, that such modifications shall be subject to appeal to the Hydraulic Appeals Board established in RCW 75.20.130.



HYDRAULIC PROJECT APPROVAL

RCW 75.20.100 or RCW 75.20.108

State of Washington
Department of Fish and Wildlife
Region 7 Office
600 Capitol Way North, MS 3155
Olympia, Washington 98501-1091

DATE OF ISSUE: August 17, 1998

LOG NUMBER: 00-D6521-01

APPEALS - GENERAL INFORMATION

IF YOU WISH TO APPEAL A DENIAL OF OR CONDITIONS PROVIDED IN A HYDRAULIC PROJECT APPROVAL, THERE ARE INFORMAL AND FORMAL APPEAL PROCESSES AVAILABLE.

A. INFORMAL APPEALS (WAC 220-110-340) OF DEPARTMENT ACTIONS TAKEN PURSUANT TO RCW 75.20.100, 75.20.103, 75.20.106, AND 75.20.160:

A person who is aggrieved or adversely affected by the following Department actions may request an informal review of:

- (A) The denial or issuance of a HPA, or the conditions or provisions made part of a HPA; or
- (B) An order imposing civil penalties.

It is recommended that an aggrieved party contact the Area Habitat Biologist and discuss the concerns. Most problems are resolved at this level, but if not, you may elevate your concerns to his/her supervisor. A request for an INFORMAL REVIEW shall be in WRITING to the Department of Fish and Wildlife, 600 Capitol Way North, Olympia, Washington 98501-1091 and shall be RECEIVED by the Department within 30-days of the denial or issuance of a HPA or receipt of an order imposing civil penalties. The 30-day time requirement may be stayed by the Department if negotiations are occurring between the aggrieved party and the Area Habitat Biologist and/or his/her supervisor. The Habitat Protection Services Division Manager or his/her designee shall conduct a review and recommend a decision to the Director or its designee. If you are not satisfied with the results of this informal appeal, a formal appeal may be filed.

B. FORMAL APPEALS (WAC 220-110-350) OF DEPARTMENT ACTIONS TAKEN PURSUANT TO RCW 75.20.100 OR 75.20.106:

A person who is aggrieved or adversely affected by the following Department actions may request an formal review of:

- (A) The denial or issuance of a HPA, or the conditions or provisions made part of a HPA;
- (B) An order imposing civil penalties; or
- (C) Any other "agency action" for which an adjudicative proceeding is required under the Administrative Procedure Act, Chapter 34.05 RCW.

A request for a FORMAL APPEAL shall be in WRITING to the Department of Fish and Wildlife, 600 Capitol Way North, Olympia, Washington 98501-1091, shall be plainly labeled as "REQUEST FOR FORMAL APPEAL" and shall be RECEIVED DURING OFFICE HOURS by the Department within 30-days of the Department action that is being challenged. The time period for requesting a formal appeal is suspended during consideration of a timely informal appeal. If there has been an informal appeal, the deadline for requesting a formal appeal shall be within 30-days of the date of the Department's written decision in response to the informal appeal.

C. FORMAL APPEALS OF DEPARTMENT ACTIONS TAKEN PURSUANT TO RCW 75.20.103 or 75.20.160:

A person who is aggrieved or adversely affected by the denial or issuance of a HPA, or the conditions or provisions made part of a HPA may request a formal appeal. The request for FORMAL APPEAL shall be in WRITING to the Hydraulic Appeals Board per WAC 259-04 at Environmental Hearings Office, 4224 Sixth Avenue SE, Building Two - Rowe Six, Lacey, Washington 98504; telephone 360/459-6327.

D. FAILURE TO APPEAL WITHIN THE REQUIRED TIME PERIODS RESULTS IN FORFEITURE OF ALL APPEAL RIGHTS. IF THERE IS NO TIMELY REQUEST FOR AN APPEAL, THE DEPARTMENT ACTION SHALL BE FINAL AND UNAPPEALABLE.



**Memorandum of Understanding
Regarding
Purpose and Arrangement for the Transfer of Real Property**

We, the undersigned, agree that the intent and purposes of Section B.2, B.6.3, B.7.1 and B.7.2 of the Interlocal Agreement regarding Public Trail Construction, Maintenance, and Operation, is for mutual benefit, compensation, and consideration in the following: the construction of a public restroom facility; the transfer from the Port to the County of an easement for the purpose of constructing a recreational trail; the transfer of the County's interest in the constructed restroom facility to the Port; the transfer from the County to the City, the County's interest in the trail easement; and the availability to trail users of the restroom facility and maintenance of said facility by the Port.

Lloyd Cahoon, Manager
Port of Port Townsend

Date

Tim McMahan, City Attorney
City of Port Townsend

Date

Klara A. Fabry, Public Works Director
Jefferson County

Date



STATE OF WASHINGTON
COUNTY OF JEFFERSON

In the Matter of

x

Resolution No. 58-95

The Exchange of
Privately Owned Real Property
For County Owned Real Property;
Larry Scott Memorial Park;
Port Townsend Paper Corporation

x

x

x

x

x

WHEREAS, Jefferson County is the owner of certain real property containing approximately 7.75 acres which is not necessary to the future foreseeable needs of the County, the description of which is set out on Attachment "A"; and

WHEREAS, Port Townsend Paper Corporation is the owner of certain real property containing approximately 10 acres, the description of which is set out on Attachment "B"; and said real property consists of railroad right-of-way which provides a vital link in the Larry Scott Memorial Park between the Port of Port Townsend and Mill Rd., and

WHEREAS, the independent appraisals conducted for the purpose of establishing the value of the said parcels, included herein as attachment "C", concluded that the value of Jefferson County's real property is \$56,000 and the value of Port Townsend Paper Corporation's real property is \$57,000 and

WHEREAS, Port Townsend Paper Corporation will accept the county parcel with the current deed restrictions limiting its use to park purposes only and will provide public access to that portion of the parcel adjacent to the railroad right-of-way, and

WHEREAS, Jefferson County will provide Port Townsend Paper Corporation or its assignee with an easement for future use of a single railroad track between the Port of Port Townsend and the paper mill, and

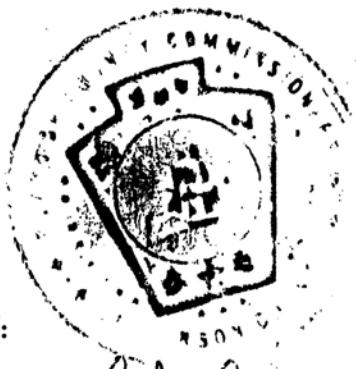
WHEREAS, Jefferson County Board of Commissioners is desirous of exchanging the property described in Attachment "B" for the property described in Attachment "A", and it appears that it is in the best interests of Jefferson County to exchange the said parcels of property; and

WHEREAS, the exchange of County-owned real property for private property for park or recreational purposes is authorized by the laws of 1965, 1st Ex. Sess., Ch 21 RCW 36.34.340;

NOW THEREFORE IT IS HEREBY RESOLVED that the Jefferson County Board of Commissioners should proceed to execute the exchange of the parcels of real property described in Attachments "A" and "B" and "C" herein.

DATED this 17th day of July, 1995.

SEAL:



ATTEST:

Lorna L. Delaney
Lorna L. Delaney
Clerk of the Board

JEFFERSON COUNTY
BOARD OF COMMISSIONERS

Robert H. Hinton, Member

Richard E. Wojt, Member

Glen Huntingford, Chairman

Presented by: *Mium Rubin-Crump*
Mium Rubin-Crump, P.W. Project Coordinator

APPROVED AS TO FORM ONLY:

Paul S. McMurtry, Esq., for
David Skeen, Prosecuting Attorney

Attachment A

PARCEL A:

All that portion of Tax No. 7 lying Northerly and Westerly of the right of way of the former Chicago, Milwaukee, St. Paul and Pacific Railroad Company, as conveyed by deed recorded June 28, 1919 in Volume 87 of Deeds, page 92, in Section 15, Township 30 North, Range 1 West of the Willamette Meridian, the whole of said Tax No. 7 being described as follows, to-wit: Commencing at a point North $35^{\circ} 22'$ East 728 feet distant from a copper bolt set in a squared stone on the South line of the Briggs Donation Claim; thence North $50^{\circ} 30'$ West 499.60 feet; thence North $39^{\circ} 30'$ East 1440.10 feet; thence South $50^{\circ} 30'$ East 300.30 feet; thence South $39^{\circ} 30'$ West 380.20 feet; thence South $50^{\circ} 30'$ East 324.70 feet to the meander line; thence Southwesterly following said meander line to a point which is South $50^{\circ} 30'$ East from the point of beginning; thence North $50^{\circ} 30'$ West to the point of beginning;

EXCEPT that portion described as follows:

Commencing at a point being North $39^{\circ} 30'$ East and distant 10 chains from a certain stake, said stake being North $35^{\circ} 22'$ East (var $22^{\circ} 30'$) and 11.03 chains distant from a copper bolt set in a squared stone on the South boundary of the Briggs Donation Claim; thence North $50^{\circ} 30'$ West 130 feet; thence North $39^{\circ} 30'$ East 5 chains; thence South $50^{\circ} 30'$ East 5 chains to the meander line of beach; thence South 42° West along meander line 5 chains to point opposite point of beginning; thence North $50^{\circ} 30'$ West to the point of beginning.

PARCEL B:

The Southwesterly 380.20 feet of the following described property:

A portion of that certain tract described in a deed dated December 29, 1914, from Henry Landes, Trustee, and Christina Landes, his wife, to Hill Estate Co., a corporation, and recorded in the office of the county auditor of Jefferson County in book 83 of Deeds at page 284, said portion hereby conveyed being described as follows, to wit:

Beginning at the intersection of the boundary line of the said tract which is described as running South $51^{\circ} 20'$ East and 324.70 feet more or less long with the Northwesterly boundary of the right of way now occupied by the second party; running thence North $51^{\circ} 20'$ West along said boundary of the intersection of said boundary with the boundary of the said tract which is described as running South $38^{\circ} 40'$ West and being 380.20 feet more or less long; thence North $38^{\circ} 40'$ East along the said boundary 380.20 feet more or less to an intersection with the boundary of the said tract which is described as running South $51^{\circ} 20'$ East and being 300.30 feet more or less long;

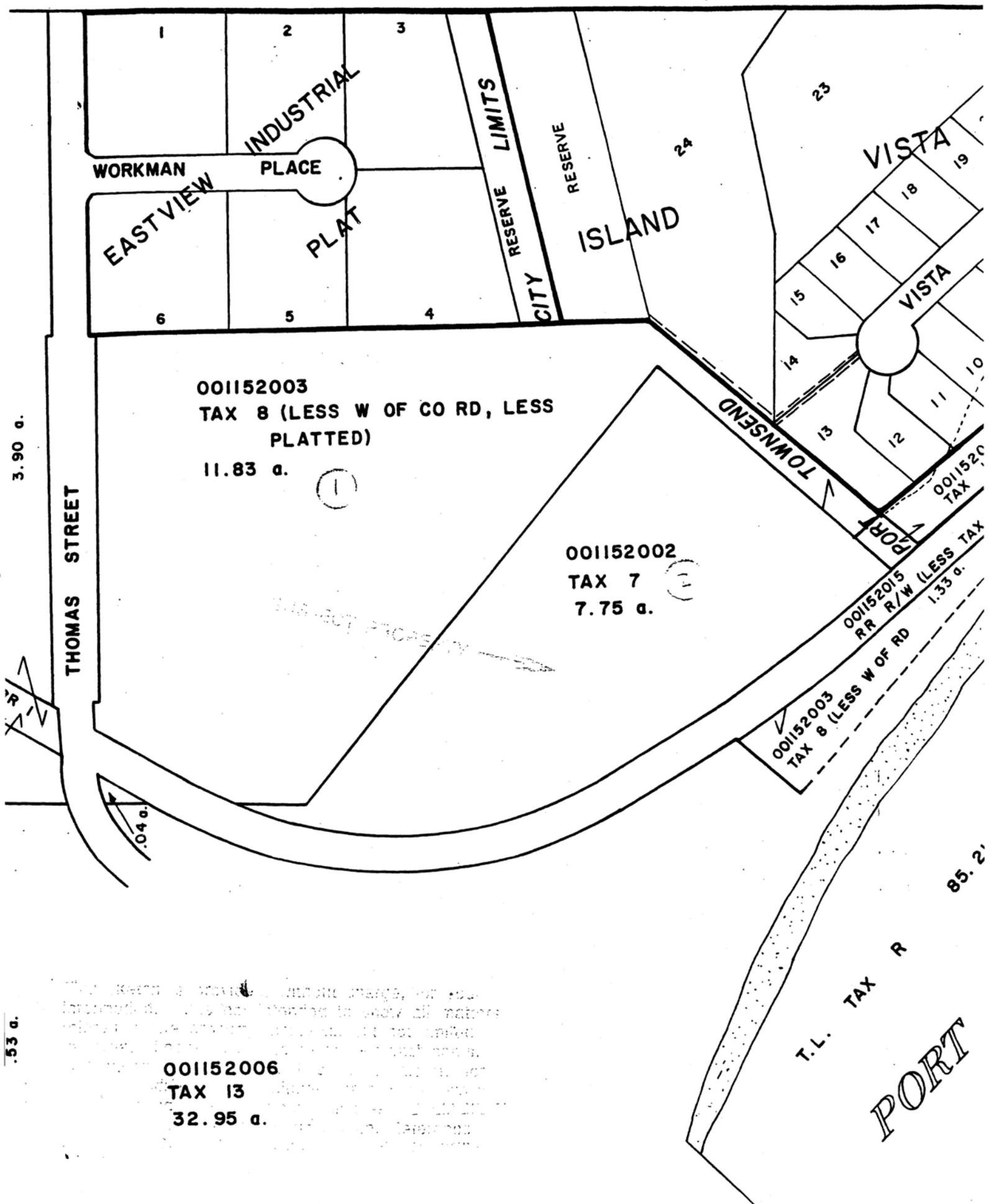
-continued-

thence South $51^{\circ} 20'$ East along the last mentioned boundary extended to a point distant 110 feet in a Northwesterly direction from the center line of the railroad track now occupied by the second party; thence in a Northeasterly direction on a line parallel to and distant 110 feet in a Northwesterly direction from the center line of the said railroad track to an intersection with the East boundary line of said tract;

thence in a Southerly direction along the East boundary line of said tract to the Northwesterly line of said railroad right of way; thence in a Southwesterly direction along the Northwesterly line of the said railroad right of way to the place of beginning; being a portion of the Briggs Donation Claim in Section 15, Township 30 North, Range 1 West, W.M.

Situate in the County of Jefferson, State of Washington.

(SW 1/4 10-30N-1W)



ATTACHMENT B

That portion of the Briggs Donation Claims in Sections 15 and 16, Township 30 North, Range 1 West of the Willamette Meridian described as follows:

Commencing at a stake bearing North 34 degrees 49' East (Var. 22 degrees 30' East) 728 feet distant from copper bolt set in a squared stone on the South boundary of the Brigg's Donation Claim
(said point of commencement being a point on the Southerly boundary of what is known as the "Brigg's Reserve," created by that certain deed given by Albert Briggs and wife to Charles Eisenbeis, dated May 10, 1888, and of record in the office of the County Auditor in Book 13, page 478; records of deeds);
thence following the boundaries of said Reserve North 51 degrees 20' West 499.60 feet, more or less, to its Westerly corner;
thence North 38 degrees 40' East 1,440.10 feet, more or less, to its Northerly corner;
thence South 51 degrees 20' East 300.30 feet, more or less, to its Easterly corner;
thence South 38 degrees 40' West 380.20 feet, more or less, to another corner;
thence South 51 degrees 20' East 324.70 feet, more or less, to the meander line of Port Townsend Bay;
thence along said meander line in a Northerly direction to its intersection with the West line of the land sold by Jefferson County to John Carlile, by deed recorded in the office of the Auditor of said County in book 55 of deeds, at page 108;
thence North along the West line of said Carlyle land 360 feet, more or less, to the North line of said donation claim;
thence along the North line of said claim and on line between Sections 10 and 15, South 89 degrees 50' West to the corner of Section 9, 10, 15 and 16, Township 30 North, Range 1 West of the Willamette Meridian;
thence South 89 degrees 14' West on a line between Section s 9 and 16, 2,641 feet, more or less, to the 1/4 section corner between said Sections 9 and 16;
thence following the 1/4 section line South 1,251 feet, more or less, to extension of the North line of a tract conveyed by Albert Brigg's and wife to F. M. Terry and Mary L. Smith;
thence North 89 degrees 20' East along said North line 2,663 feet, more or less, to the Northeast corner of said Terry and Smith tract;
thence along the East line of said tract South (40 degrees East) 924 feet, more or less, to another corner of said Terry and Smith tract of land;
thence North 89 degrees 20' East 638.90 feet, more or less, to said meander line;
thence along said meander line North 31 degrees 20' East 154.60 feet, more or less, to a point bearing South 51 degrees 20' East 100.70 feet, from the initial point;
thence on a direct line to the place of beginning;
EXCEPTING all that portion thereof lying South of the extension of the said North line of the tract conveyed by Albert Briggs and wife, to F. M. Terry and Mary L. Smith;
EXCEPTING ALSO one acre of land conveyed by Albert Briggs and wife, to Jacob A. Chapman recorded in Book 8 of Deeds, at page 560, records of said County;

-continued-

AND EXCEPTING also so much, if any, of that certain parcel of land containing 0.82 acres more or less, conveyed by said Briggs and wife, to Norman R. Smith by deed recorded in Book 9 of Deeds at page 218, records of said County, as may lie within said described tract; EXCEPTING right of way of that portion of the former Chicago, Milwaukee, St. Paul and Pacific Railroad Co. right-of-way, AND EXCEPTING the tract conveyed to Port Townsend and Puget Sound Railway Company by three deed recorded in Volume 85 of Deeds, at page 601, 602, and 603, records of said County, AND EXCEPTING the water pipe line right of way, acquired by the City of Port Townsend by Commissioners Proceedings Volume H, Pages 254-55; AND EXCEPTING land deeded to Jefferson County for road purposes by Kunigunda K. Froberg by instrument recorded under Auditor's File No. 87333, records of said County; AND EXCEPTING portion condemned for water pipe line described in Volume 96 of Deeds, Pages 235-36; AND EXCEPTING portion thereof East of Paper Mill Road (CR No. 44). Situate in the County of Jefferson, State of Washington. (Also know as Tax No. 8 in Section 15 and Tax No. 22 in Section 16, Township 30 North, Range 1 West, W.M.)

Situate in the County of Jefferson, State of Washington.

JEFFERSON TITLE COMPANY INC.
2205 WASHINGTON STREET
P.O. BOX 256
PORT TOWNSEND, WA 98368
(360) 385-2000

JEFFERSON COUNTY PUBLIC WORKS
ATTN: MIUM RUBIN-CRUMP

SELLER : PT PAPER CORP

PURCHASER : JEFFERSON COUNTY

OUR ORDER NO.: 45341-RB

SUPPLEMENTAL NO. 1 TO THE FIRST COMMITMENT

**ATTENTION: THIS SUPPLEMENTAL CONTAINS CHANGES WHICH IMPACT TITLE
TO PROPERTY SET FORTH IN THE ABOVE-REFERENCED COMMITMENT.**

S C H E D U L E "B"

X Paragraph 2 of the preliminary commitment is changed as follows:

2. General taxes, as follows, together with interest, penalty and statutory foreclosure costs, if any, after delinquency:

<u>Tax Account No.</u>	<u>Year</u>	<u>Amount Billed</u>	<u>Amount Paid</u>	<u>Principal Balance</u>
# 001 152 015	1995	NO AMOUNT GIVEN		
# 001 161 002	1995	\$3,288.28	\$1,644.14	\$1,644.14
# 001 152 008	1995	\$583.42	\$291.71	\$291.71

X NOTE 3 of the preliminary commitment is changed as follows:

NOTE 3:

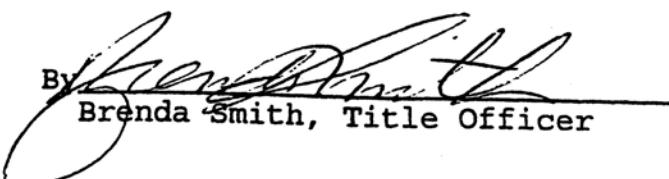
It is our understanding that the following is to be included in the forthcoming documents. It is however, expressly excluded from our insurance coverage:

TOGETHER WITH that portion of the former Chicago, Milwaukee, St. Paul and Pacific Railroad Company's right of way in the Northwest Quarter of Section 15, Township 30 North, Range 1 West, W.M., as conveyed by deed recorded October 15, 1987 under Auditor's File No. 310886, records of Jefferson County, Washington.

X Except as to the matters reported hereinabove, the title to the property covered by this order has NOT been reexamined.

Dated as of June 5, 1995 at 8:00 A.M.

JEFFERSON TITLE COMPANY INC.

By 
Brenda Smith, Title Officer

BS/dss

cc: RH Winters Co./Bob Winters

IDENTIFICATION OF THE PROPERTY:

The subject property is a portion of the abandoned right of way for the Chicago, Milwaukee, St. Paul and Pacific Railroad Company that meanders through the Port Townsend Paper Company Property from the eastern boundary of the Northwest Quarter of Section 16, Township 30 North, Range 1 West, W.M. to the western boundary of the Hill Avenue Water Way in Port Townsend, Washington.

LEGAL DESCRIPTION:

The property is legally described as follows:

That portion of the abandoned right of way for the Chicago, Milwaukee, St. Paul and Pacific Railroad Company that lies in the Northeast Quarter of Section 16; the Northwest Quarter of Section 15; and the Southeast Quarter of Section 10 lying southwesterly of the Hill Avenue Water Way, all in Township 30 North, Range 1 West, W. M. Situated in Jefferson County, Washington.

PURPOSE OF THE APPRAISAL:

The purpose of this appraisal is to estimate the fair market value of the above identified property.

FUNCTION OF THE APPRAISAL:

It is our understanding that this appraisal will be used as an independent measure of the value of the property to facilitate a sale or exchange of the subject property.

SE 1/4 9-30N-1W

2728.92

16 9
15 10

CLAIM

DONATION

LAND

(NW 1/4 15-30N-1W)

001161002
TAX 22
RR 2

MILL ROAD

001161002 TAX 22 .85 a.

P.T. Paper Corp.

carried in 15-30N-1W

001161002
TAX 22
4.75 a.

(NW 1/4 16-30N-1W)

001162002
TAX 2
1.92 a.

001161002 TAX 22 .63 a.
001161003
TAX 26
1.36 a.

1

001161001
TAX 5 (LESS
90' W OF
PIPELINE) .91 a.

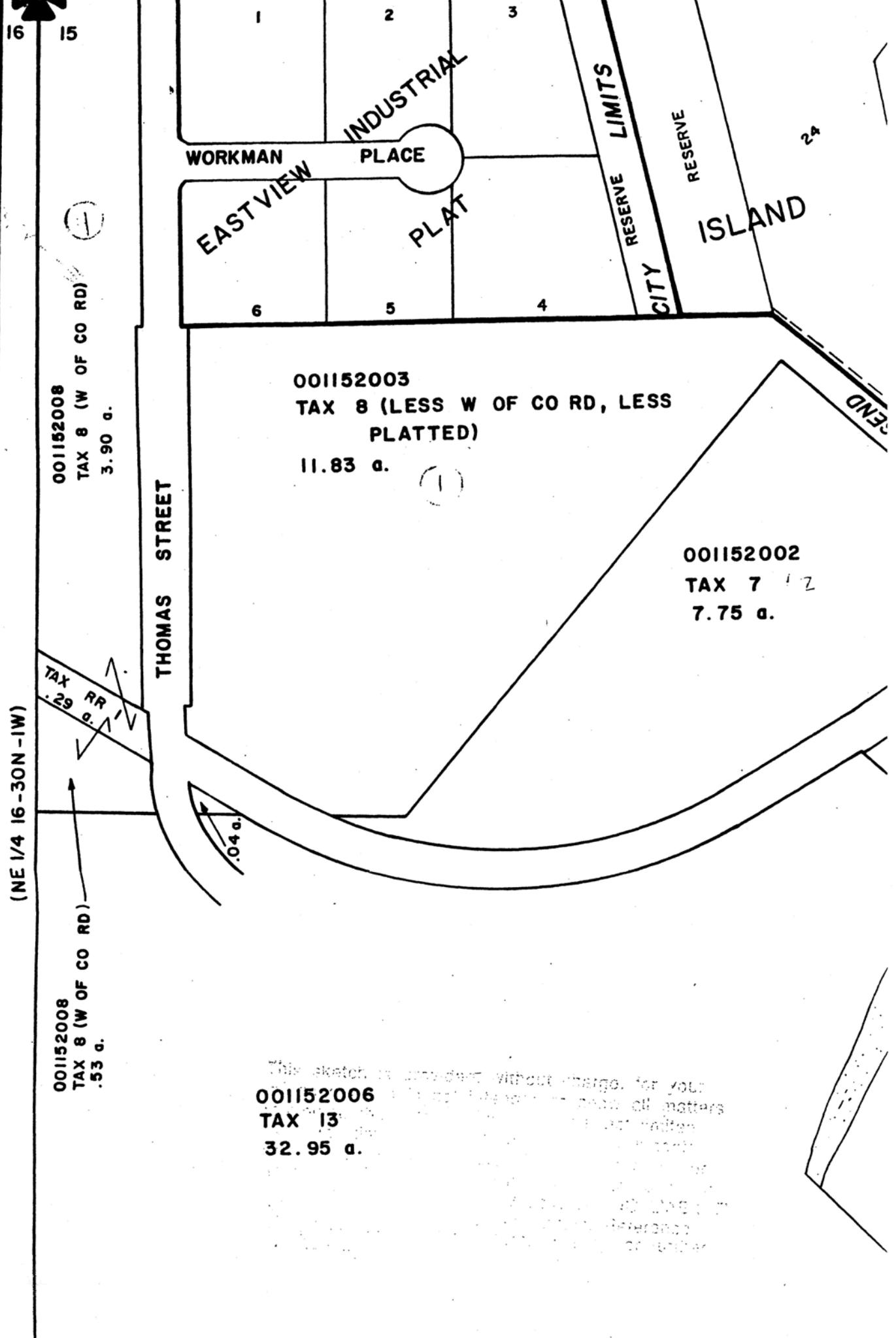
001162005 TAX 5 (90' W
OF PIPELINE) .72 a.
001161001
.31 a.

2ND
BÖREN AVE.

1003

.26
a.

001161003
TAX 26
62.81 a.
(25 a. IN LAGOON)



SEC. 10, 15 & 16, T. 30 N., R. 1 W., W.M.

MATCH LINE "D"

MATCH LINE "D"

BR/GGS D.L.C.

VOLUME 6 OF ISLAND VISTA
PORT TOWNSEND AND PUGET SOUND RAILROAD

PLATS, PAGE 69
N 42° 33' E

895.36
106.00
N 42° 33' E

TIDELAND NO. 113
TIDE LAND CONCURRENT MEANDER LINE

LEGEND

- SET 5/8" REBAR WITH PLASTIC SURVEY CAP STAMPED "NTI-WALTERS PLS 22044".
- ▲ FOUND 5/8" REBAR WITH PLASTIC SURVEY CAP STAMPED "NTI-PLS 22344" PER VOLUME 3 OF SURVEYS, PAGE 51.
- SET 1/2" MAIL WITH PLASTIC SURVEY CAP STAMPED "NTI-WALTERS PLS 22044".

NOTE: THE CENTERLINE OF THE RAILROAD THROUGH SECTION 10 AND TIDELAND TAX DISTRICT NO. 112 WAS COMPUTED PER THE ALIGNMENT SHOWN ON ORIGINAL ALIGNMENT NOTES SHOWN ON RAILROAD COMPANY MAP DATED AUGUST 1923. THIS ALIGNMENT DOES NOT FIT THE EXISTING RAILS. NO ALIGNMENT OR RIGHT-OF-WAY WIDTH WERE SPECIFIED IN THE LEGAL DESCRIPTIONS CONVEYING THE RAILROAD TO THE PORT TOWNSEND PAPER COMPANY THROUGH SECTION 10 AND TIDELAND TAX DISTRICT NO. 112.
SEE SHEET 1 FOR ADDITIONAL NOTES.

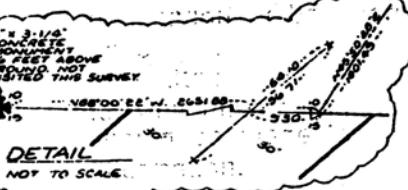
NO.	BEARING	DISTANCE
1	N 32° 51' 25" E	4.75
2	N 31° 03' 17" E	50.89
3	N 33° 53' 06" E	55.11
4	N 18° 51' 51" E	68.81
5	N 0° 25' 55" E	67.25
6	N 70° 03' 3" E	55.10
7	N 18° 55' 40" E	63.39
8	N 25° 27' 06" E	68.77
9	N 69° 07' 54" E	60.96

WASHINGTON COORDINATE SYSTEM OF 1927

HILL AVENUE WATERWAY

Information is provided, without charge, for your information. It is not intended to be definitive, detailed or complete. It is not to be used for engineering purposes.

Scale in feet



DESIGNED BY:
DRAWN BY: KARENNAH HAMMONDS
PLAT CHECK BY: X M H
FINAL CHECK BY: M

PORT TOWNSEND PAPER COMPANY SURVEY



NORTHWESTERN TERRITORIES, INC.
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Construction Contractors • Geologic Survey
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